

# Magill Village Partnership

## MASTERPLAN REPORT

25 OCTOBER 2013



**COMPLETE**



# Contents

## 1.0 INTRODUCTION

- 1.1 Background
- 1.2 Study Area
- 1.3 Background Documents
- 1.4 Purpose of the Report and Project Objectives
- 1.5 Acknowledgments

## 2.0 CONTEXT

- 2.1 History
- 2.2 Heritage

## 3.0 STUDY AREA ANALYSIS

- 3.1 Study Area Analysis

## 4.0 CONSTRAINTS AND OPPORTUNITIES

- 4.1 Constraints and Opportunities

## 5.0 VISION AND GUIDING PRINCIPLES

- 5.1 Background to Vision and Guiding Principles

## 6.0 MASTERPLAN

- 6.1 Overall Masterplan
- 6.2 Economic Activation

## 7.0 IMAGE AND GRAPHICS

- 7.1 Image and Graphics

## 8.0 STAGING

- 8.1 Staging Principles

## 9.0 CONCLUSION

- 9.1 Conclusion
- 9.2 The Next Step

## LIST OF FIGURES

- Figure 1.1 Location Plan
- Figure 1.2 Broader Precinct Features
- Figure 1.3 Precinct Plan
- Figure 1.4 Study Area (Existing Context)
- Figure 1.5 Precinct Plan: Precinct 1
- Figure 1.6 Precinct 1 (Educational Precinct: University of South Australia - Magill Campus)
- Figure 1.7 Precinct Plan: Precinct 2
- Figure 1.8 Precinct (Commercial Precinct; Magill Road)
- Figure 1.9 Precinct Plan: Precinct 3
- Figure 1.10 Precinct 3 (Typical Residential Precinct)
- Figure 1.11 Precinct Plan: Precinct 4
- Figure 1.12 Precinct 4 (Third Creek)
- Figure 1.13 Precinct Plan: Precinct 5
- Figure 1.14 Precinct 5 (Industrial Precinct)
- Figure 2.1 Transport plan of Adelaide (1878 - 1978)
- Figure 2.2 Historical Background (First Subdivision forming Magill Village - 1838)
- Figure 2.3 Heritage
- Figure 3.1 Study Area Analysis - Existing Land Use
- Figure 3.2 Study Area Analysis - Environmental
- Figure 3.3 Study Area Analysis - Built Form
- Figure 4.1 Precinct 1 Constraints and Opportunities
- Figure 4.2 Precinct 2 Constraints and Opportunities
- Figure 5.1 Vision and Guiding Principles
- Figure 5.2 Vision and Guiding Principles Plan
- Figure 6.1 Overall Masterplan
- Figure 6.2 University Masterplan
- Figure 6.3 Education Precinct (Reference Images)
- Figure 6.4 Magill Masterplan 1/3
- Figure 6.5 Magill Masterplan 2/3
- Figure 6.6 Magill Masterplan 3/3
- Figure 6.7 Pepper Mews Layout Plan 1/2
- Figure 6.8 Pepper Mews Layout Plan 2/2
- Figure 6.9 Typical Street Sections
- Figure 6.10 Photomontage: Entering Magill Village (Travelling East from City)
- Figure 6.11 Photomontage: New Pepper Mews Village Square
- Figure 6.12 Photomontage: New Pepper Mews (linking Third Creek to Magill Road)
- Figure 6.13 Photomontage: Modified Intersection (St Bernards/ Penfold Road)
- Figure 6.14 Photomontage: Entering Magill Village (Travelling West from Hills)
- Figure 6.15 Planting Palette
- Figure 6.16 Furniture and Fixtures
- Figure 6.17 Economic Value
- Figure 7.1 Graphic Image 1
- Figure 7.2 Graphic Image 2
- Figure 7.3 Graphic Image 3
- Figure 7.4 Graphic Image 4
- Figure 7.5 Graphic Image 5
- Figure 7.6 Graphic Image 6
- Figure 7.7 Graphic Image 7
- Figure 7.8 Graphic Image 8
- Figure 8.1 Staging Plan (Magill Village)
- Figure 8.2 Staging Plan (Stage 1 - Village Square)

## MAGILL VILLAGE Partnership

### **OBJECTIVE**

The *Magill Village Partnership* seeks to achieve comprehensive renewal of the Magill Village experience. It recognises that the most successful places evolve through government, business, education sector and community members partnering towards the realization of a shared vision and goals.

### **VISION**

Magill Village will support the local community's everyday recreation, social, shopping, education and business needs; offering to all who visit, an authentic, appealing and memorable experience.

# 1.0 Introduction



# Introduction

## 1.1 BACKGROUND

The Magill Village Partnership is a collaboration between the City of Burnside and Campbelltown City Council to investigate and identify the potential of the Magill Town Centre located around the intersection of Magill Road and St Bernards/Penfold Road east of the Adelaide CBD.

Both Councils have recognised that the area is rich in social, cultural heritage and presents a unique opportunity to create an exemplar 'Village' model that encompasses urban renewal streetscape improvements and urban landscape work. It can become a prototype for similar initiatives in other areas around Adelaide.

The Magill Town Centre precinct is centred around the intersection of Magill Road and St Bernards/Penfold Road in Magill. It is located approximately 6 km east of Adelaide CBD and is located within both the Campbelltown City Council and the City of Burnside Council areas. The precinct is the original Magill Village township and remnants of former town activities including the original school (Pepper St Studios), the police station (Justin Gall Real Estate), the Hall (Magill Institute) and the Hotel (Tower Hotel) can still be seen today. However, the precinct lacks a vibrant and cohesive urban character, with many disparate land uses, with poor physical connections, poor design quality and a disjointed relationship between precinct activities. The precinct is dominated by road and ad hoc car parking infrastructure, serving a predominantly car dependant community with little consideration for other street users (pedestrian, cyclist and public transit users).

It is located at the edge of the suburbs at the foot of the Adelaide Hills and due to its close proximity to Adelaide City, the precinct and its immediate environs represent a key development area for concentrated growth and intensified development under the State Government's 30-Year Plan for Greater Adelaide (30-Year Plan). This intended future growth will see the introduction of more diverse and higher density living choices, supporting a significant increase to resident population, as well as a transit mode shift away from car dependency to more sustainable transit options. The local community will rely increasingly on the town centre precinct and surrounds for daily social, business, recreational, education and retail needs. The changes envisaged under the 30-Year plan have the potential to transform the physical, social and economic character to the precinct and result in a complete urban renewal of the area.

In light of the above changes, both the Campbelltown City Council and the City of Burnside recognise the Magill Town Centre precinct as having the potential to move towards a more village style environment. Such a model will support future anticipated community growth, more sustainable living choice models and improved quality of life for local residents, workers and visitors to create a Magill Village that will act as an exemplar for other similar town centres in the Greater Adelaide region.

In June 2012, the Australian Government through the Liveable Cities Program announced a funding contribution to support the Campbelltown City Council and the City of Burnside Council to develop, in partnership with the University of South Australia, a Master Plan for the Magill Town Centre precinct that incorporates the principles of the 30-Year Plan and the aspirations of the two Councils and the local community and to act as a national model for best practice. This funding was in response to an application lodged in December 2011.



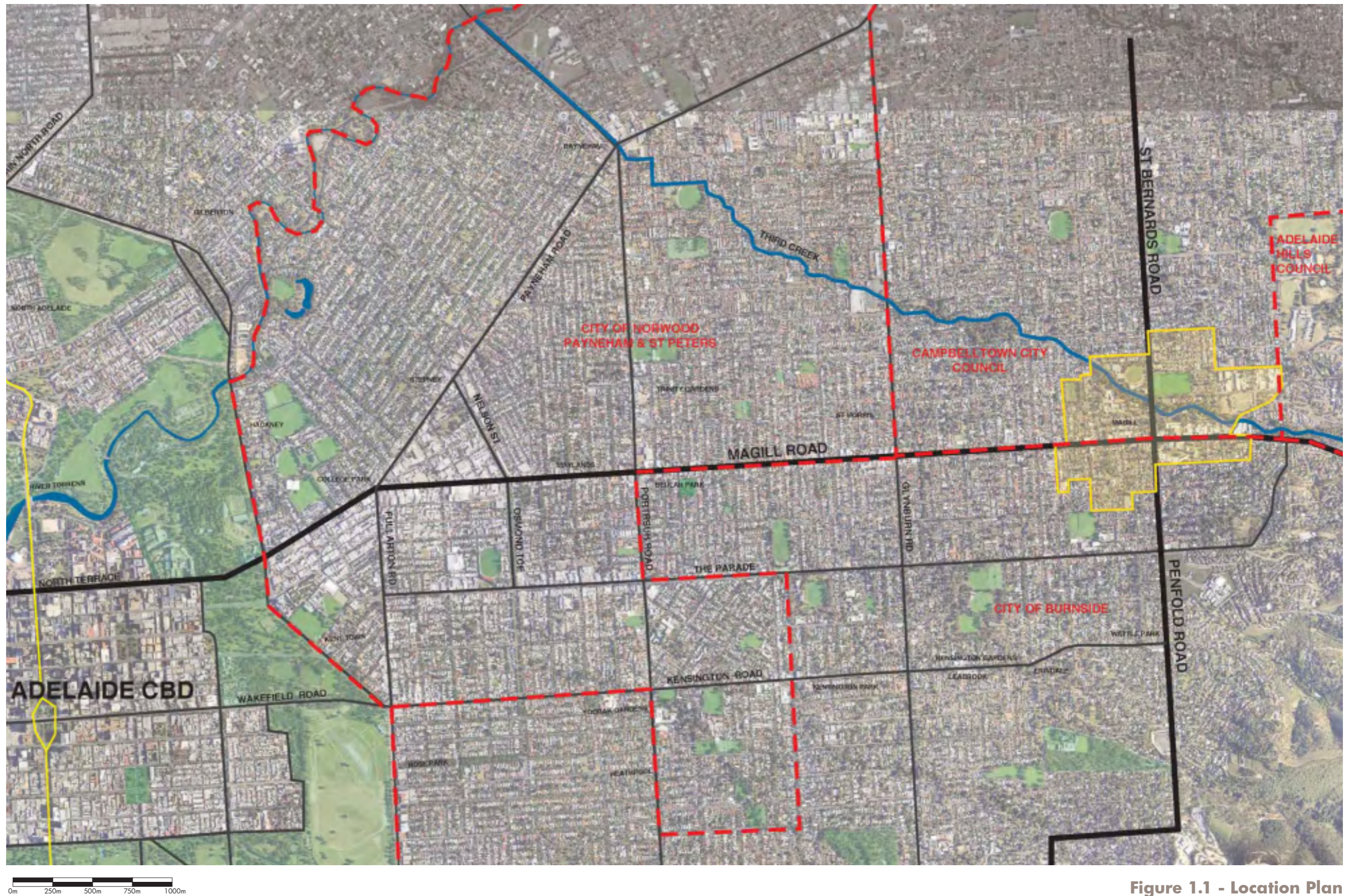


Figure 1.1 - Location Plan

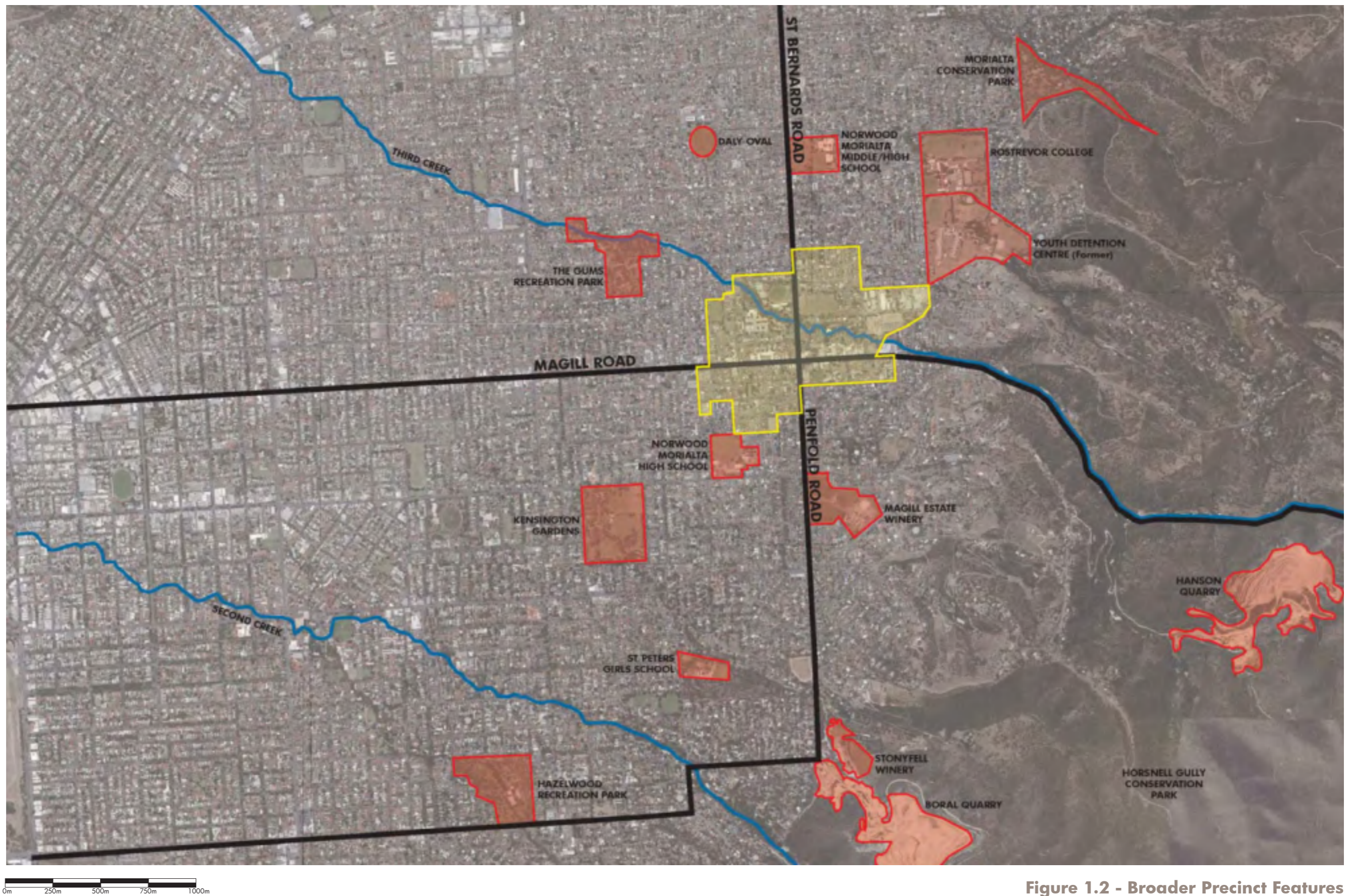


Figure 1.2 - Broader Precinct Features

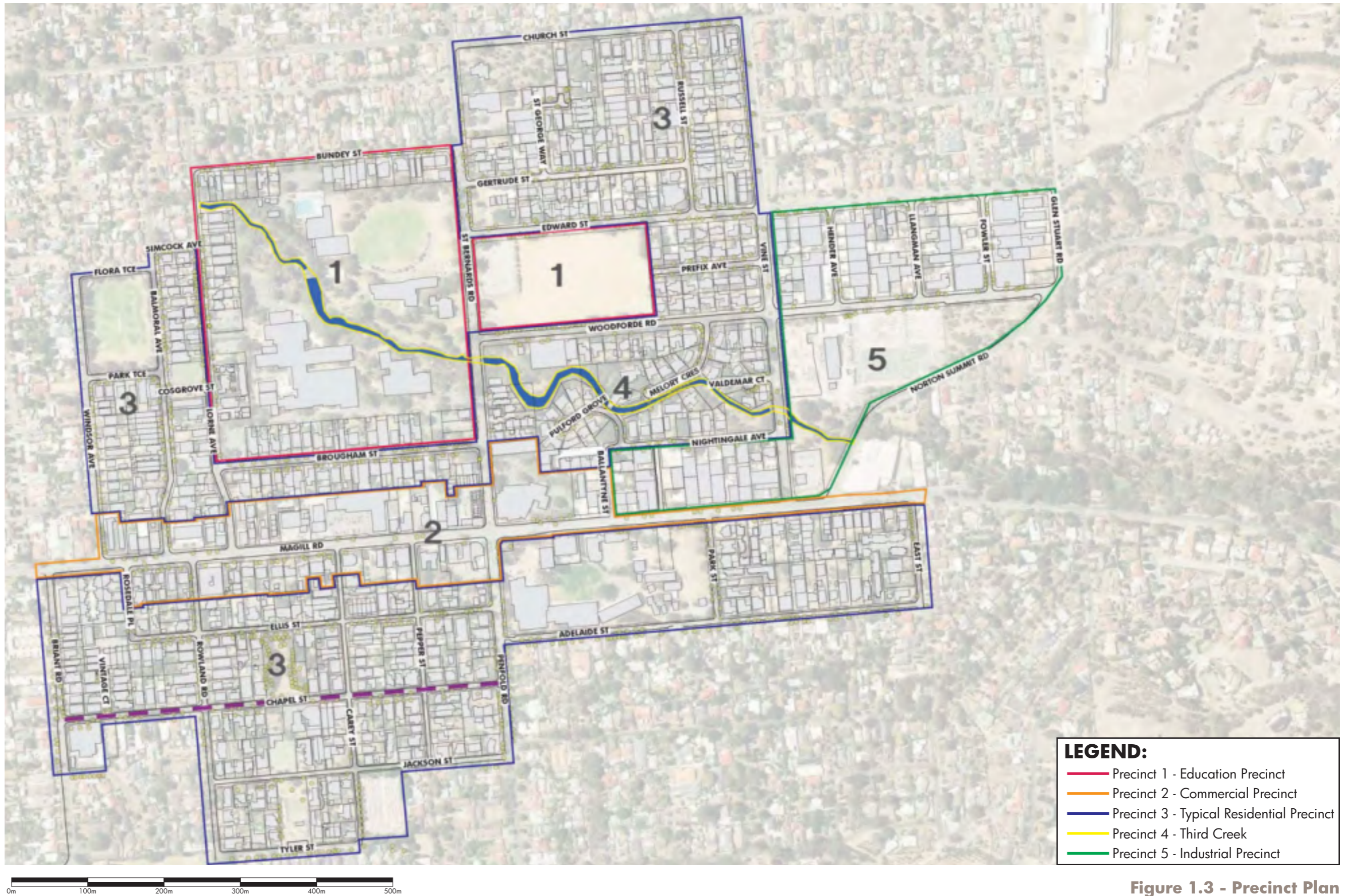


## 1.2 STUDY AREA

The Study Area is approximately centred about the intersection of Magill Road and St Bernards/ Penfold Road and extends north, south, east and west from the intersection. The study area is large, measuring approximately 1 km x 1 km. Consequently it is very diverse with a large range of varying characteristics and land uses. Its land uses comprise light industry, community, recreational, residential, commercial and educational. For this report, the boundaries of the study area have been established so they can adequately capture the key features of each of these land uses in order to establish how they can contribute to the overall quality and amenity of the precinct. Following a high level assessment of the overall area, the study area has been divided into 5 smaller precincts that best reflect the distinct land uses. These have been identified to allow for a more detailed master planning assessment to be undertaken relevant to the particular area.

<u>Precinct</u>	<u>Predominant Use</u>
Precinct 1 - Education Precinct	University of South Australia - Magill Campus
Precinct 2 - Commercial Precinct	Commercial activity on Magill Road
Precinct 3 - Typical Residential Precinct	Residential (Chapel Street as an example)
Precinct 4 - Third Creek	Recreation / Open space
Precinct 5 - Industrial Precinct	Industrial

Each precinct does not represent the full extent of that type of activity or land use found in the overall study area. Each precinct has a wide range of different uses but the precinct boundary has been determined to encompass the predominant land use. For example, the Magill Primary School has been included in the Residential Precinct.



**LEGEND:**

- Precinct 1 - Education Precinct
- Precinct 2 - Commercial Precinct
- Precinct 3 - Typical Residential Precinct
- Precinct 4 - Third Creek
- Precinct 5 - Industrial Precinct

**Figure 1.3 - Precinct Plan**

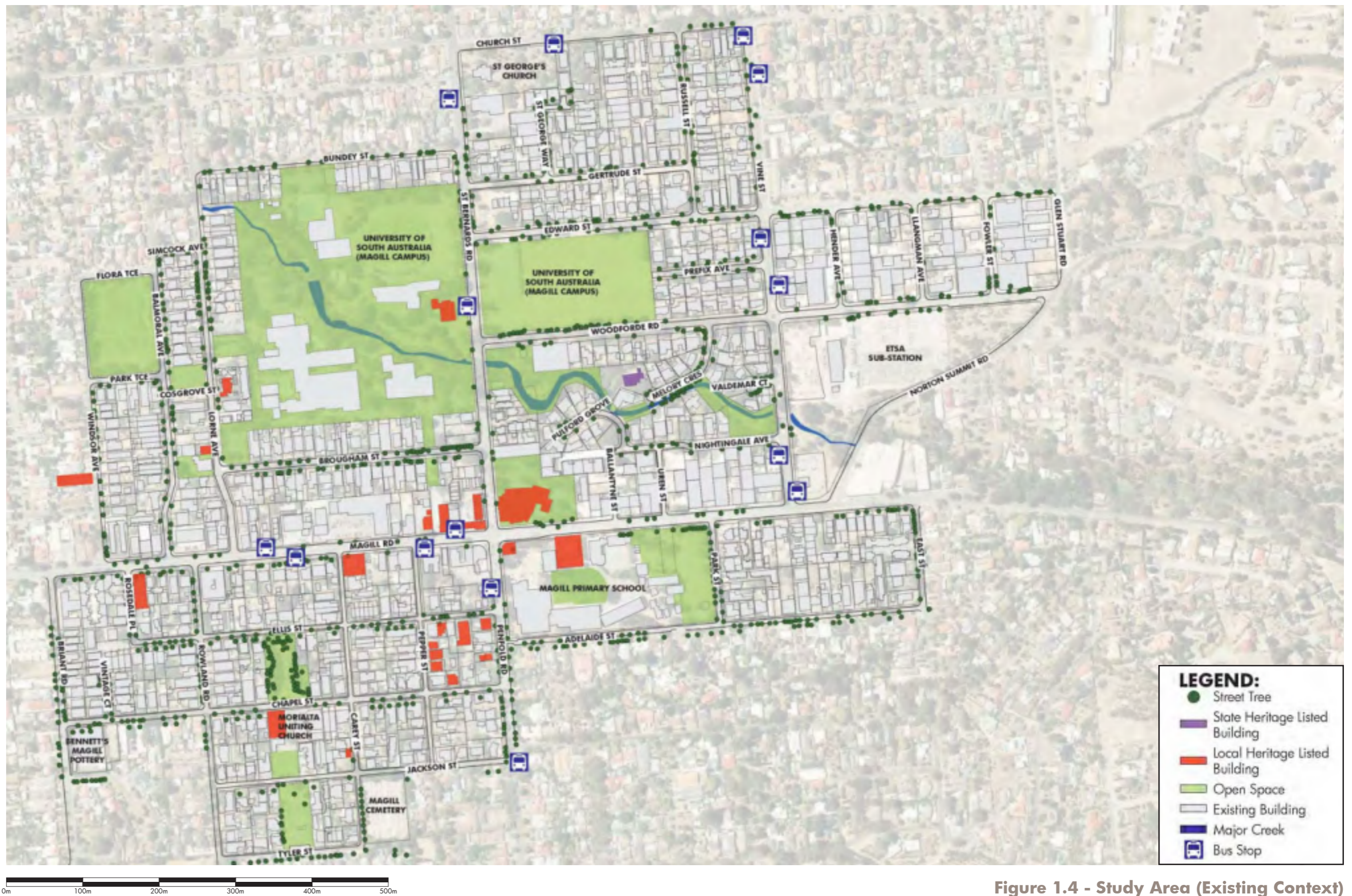


Figure 1.4 - Study Area (Existing Context)

### Precinct 1 - Education Precinct - University of South Australia - Magill Campus

As announced in the University of South Australia's Horizon 2020 Strategic Statement published in 2010, The University of South Australia has a current plan to relocate teaching spaces from the Magill Campus to the Adelaide CBD Campus, progressively up to year 2020. The University's Strategic Statement outlines opportunities to consolidate University of South Australia campuses to make better use of resources and to develop a more sustainable profile in the environment with a focus on boosting student numbers in the Adelaide CBD. At the time of finalising this report, the University of South Australia had announced that it was not looking to sell the current Magill University site, but was now considering a broad range of alternative uses and possibilities for the site, including returning it to a specialised teaching and research facility. Future proposed uses for the site can be expected to either maintain or reduce student numbers. In any event, the future changes currently being considered for the site, affords an opportunity to consider introducing other compatible uses that can contribute to the overall precinct viability and ongoing social, environment and economic sustainability.

The Magill Campus is a significant, single land parcel in the eastern suburbs and it represents one of the few remaining open spaces that is not a designated park east of the Adelaide CBD. Its economic value in a very dense urban environment is considerable and will be an attractive proposition to compatible development bodies. In addition, its social and community value is further enhanced by its natural creek setting and extensive educational building stock. Much of this is in good condition and although it is educational in nature it presents a good asset for adaptive re-use. Its use following the re-development of University of South Australia after they consolidate to the CBD will have a large impact on the overall success of the Magill Village.

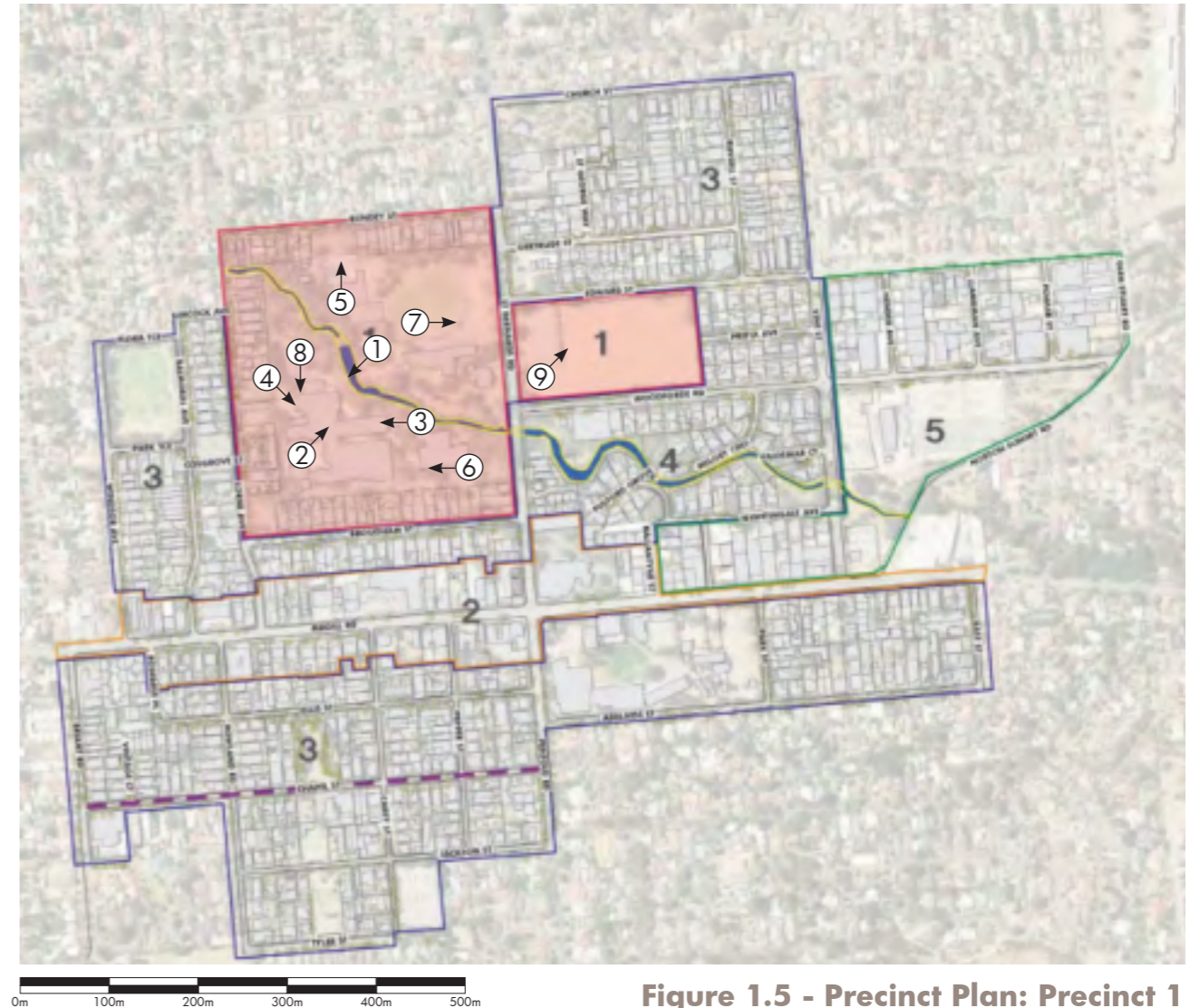


Figure 1.5 - Precinct Plan: Precinct 1

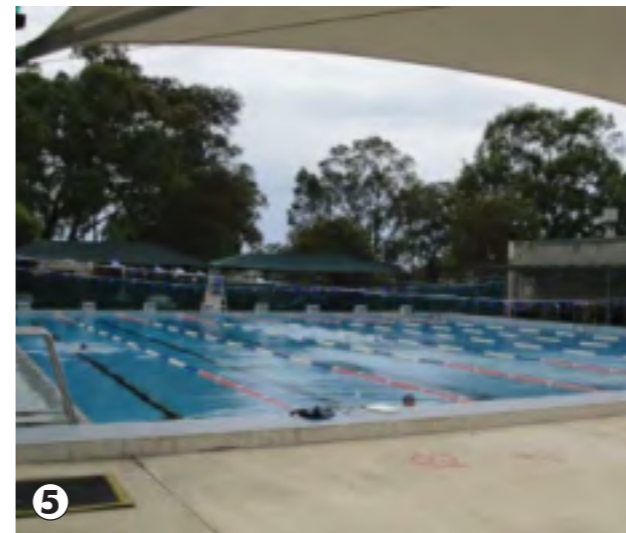


Figure 1.6 - Precinct 1 (Educational Precinct: University of South Australia - Magill Campus)

## Precinct 2 - Commercial Precinct - Magill Road

Magill Road is the main east-west street through Magill Village and one of the major east-west roads east of the city. It connects the Adelaide Hills to the Adelaide CBD. Within the study area Magill Road is currently a 2 lane road, with parallel parking on each side. It is used by a high volume of through traffic and local traffic, as well as being a main bus route east of the city. Further west of the study area Magill Road increases to a 4 lane road.

The Magill Road precinct within the study area is a slightly steeper section of Magill Road as it approaches the foothills and is the traditional town centre of the area. Whilst there are signs of the original town centre activity and layout, the town centre's nature has been heavily eroded over the years as a number of different land uses have been established and Magill Road has become dominated by vehicles.

Magill Road contains a number of heritage buildings, including The Pepper Street Arts centre, the former Magill Institute Building, the former Police Station (Justin Gall Real Estate) and the Tower Hotel.

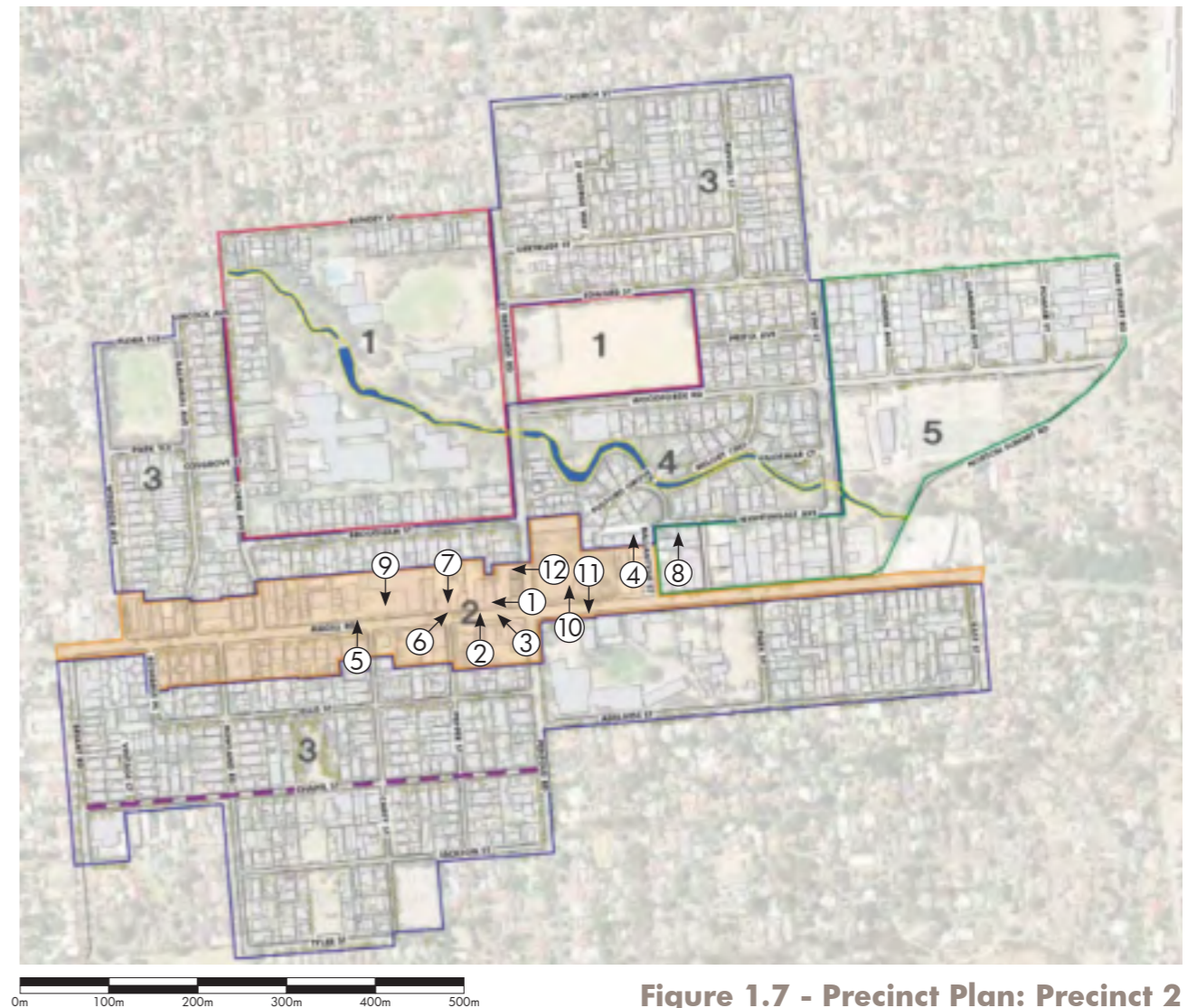


Figure 1.7 - Precinct Plan: Precinct 2

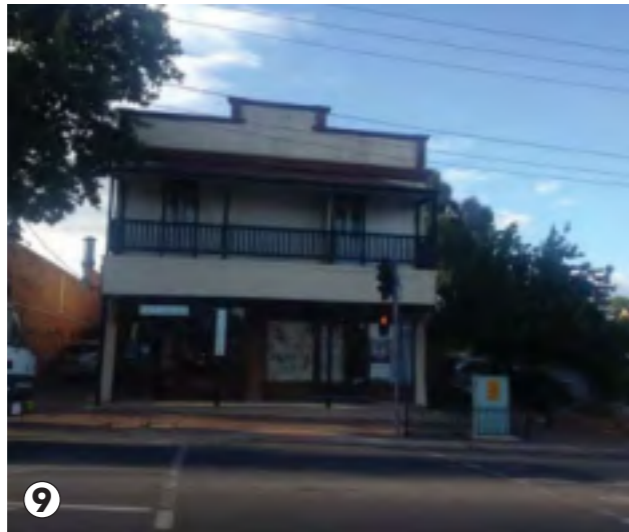
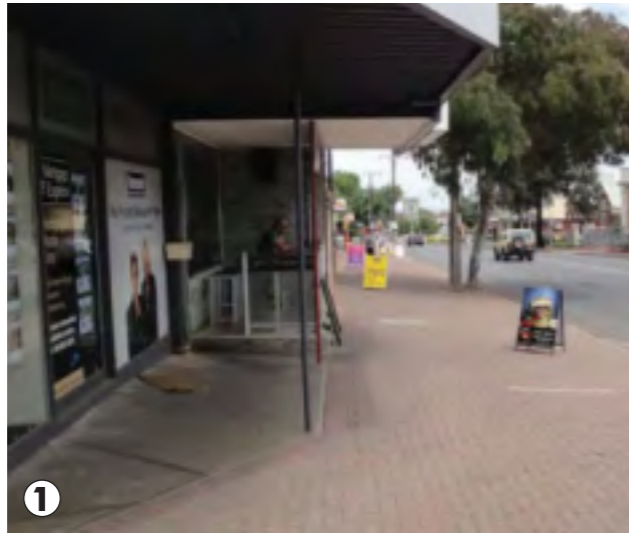


Figure 1.8 - Precinct 2 (Commercial Precinct: Magill Road)

### Precinct 3 - Typical Residential Precinct

There are a number of different types of residential streets within the study area all displaying different characteristics. Chapel Street, which runs east-west, south of Magill Road has been selected as the typical street as it encompasses a number of the qualities that are the focus of this Masterplan.

Chapel Street is a narrow east-west street that connects Penfolds Road and Bennetts Pottery. It has a number of different housing styles ranging from small traditional homes on individual residential blocks to new medium density properties. As is typical with the wider study area, Chapel Street also has a number of other land uses (Bennetts Pottery, Morialta Uniting Church, open space and a community productive garden). It is anticipated that the Masterplan objectives developed for this street will be largely applicable to other streets in the precinct.

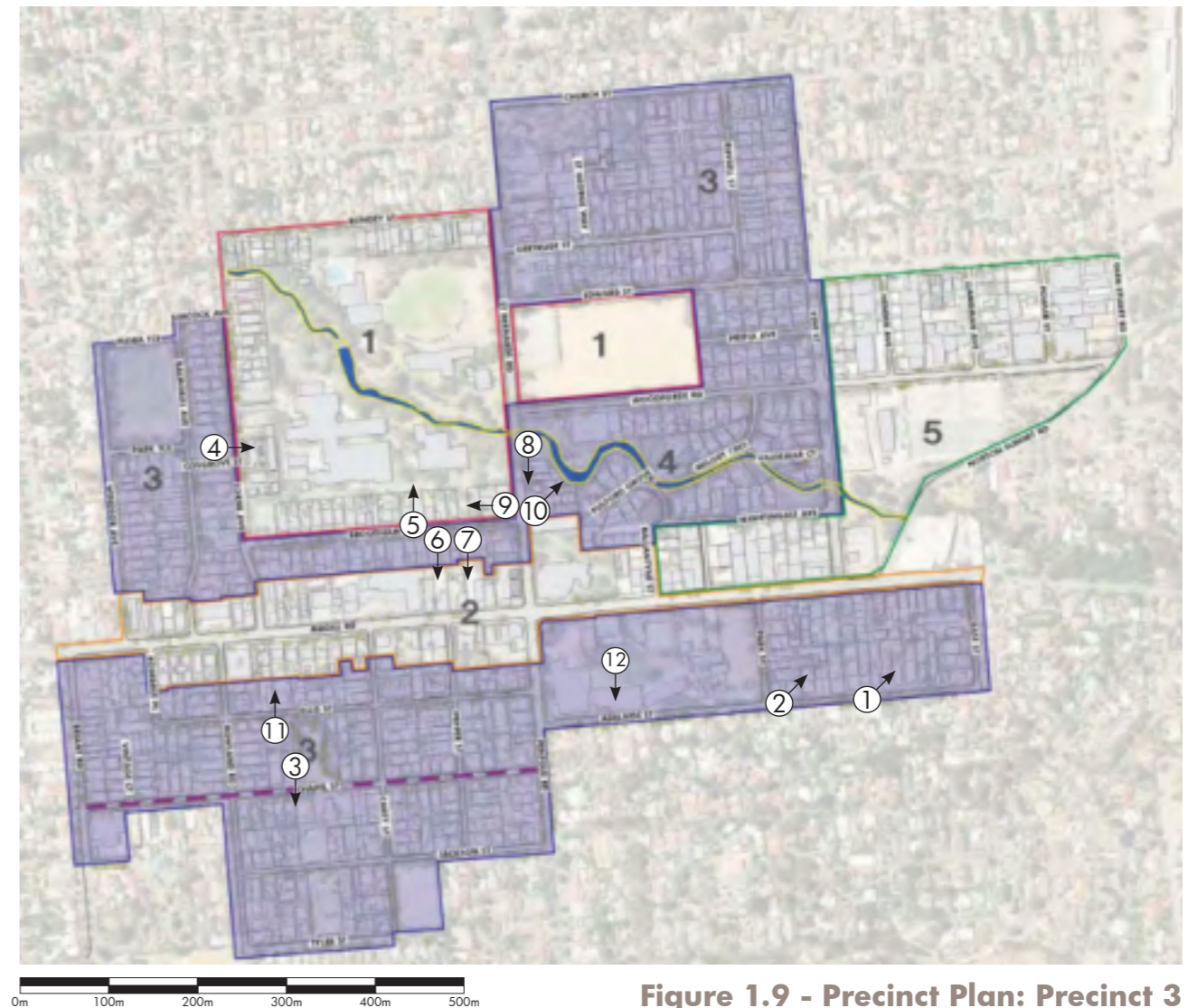


Figure 1.9 - Precinct Plan: Precinct 3





Figure 1.10 - Precinct 3 (Typical Residential Precinct)

### Precinct 4 - Third Creek

Third Creek runs from the south-east to the north-west diagonally through the study area. Like other creeks flowing through the eastern suburbs from the Adelaide Hills, Third Creek is dry for a number of months per year.

East of St Bernards Road it runs through residential areas and has limited direct access from publicly accessible areas. West of St Bernards Road it runs through the University of South Australia - Magill Campus providing the campus with a unique creek setting but also separating the site into two (2) separate parcels of land. The creek presents an ideal potential to become more integrated into the overall urban fabric and to contribute to the active and passive recreation outcomes of the precinct.

Enhancing pedestrian and visual connections to the creek from Magill Road Commercial Precinct is seen as a critical objective in supporting the commercial precinct as a destination.

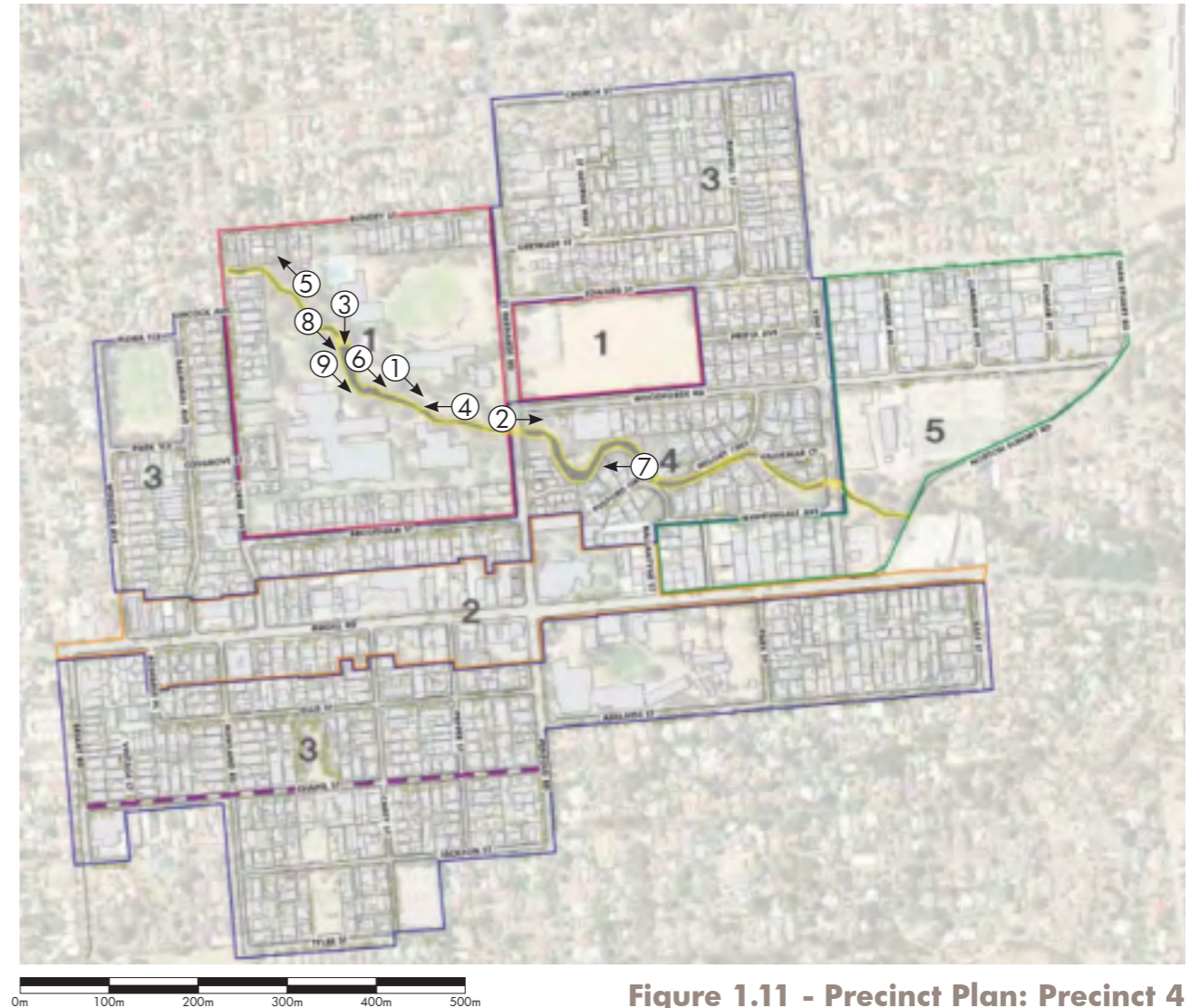


Figure 1.11 - Precinct Plan: Precinct 4

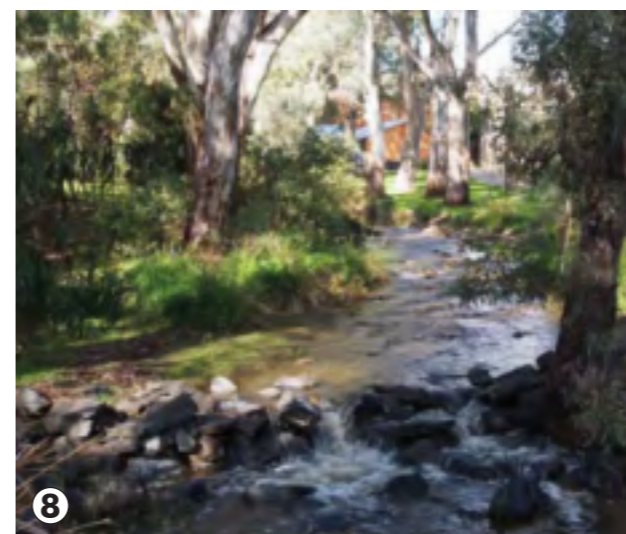


Figure 1.12 - Precinct 4 (Third Creek)

### Precinct 5 - Industrial Precinct

The study area, particularly east of St Bernards/Penfold Road accommodates a variety of industrial uses. These uses reflect in part the strong ethnic history of the precinct particularly since the Second World War and European migration into the north-eastern suburbs of Adelaide. Consequently the precinct is home to a number of pottery, ceramic, stone masons and concrete based manufacturing businesses. It also accommodates other basic industrial uses including salvage and recycling, crash repairs and motor related business. Many of these front directly onto Magill Road, possibly reflecting the relatively cheap land prices that were available in the past and convenient vehicle access to a major traffic route well connected to the CBD.

A large portion of this precinct is occupied by the SA Power Networks sub-station.

Opportunities exist to foster collaboration between the traditional manufacturing businesses, with a broader based creative industries focus that celebrates the ethnic nature of some of the industries that currently exist in the precinct.

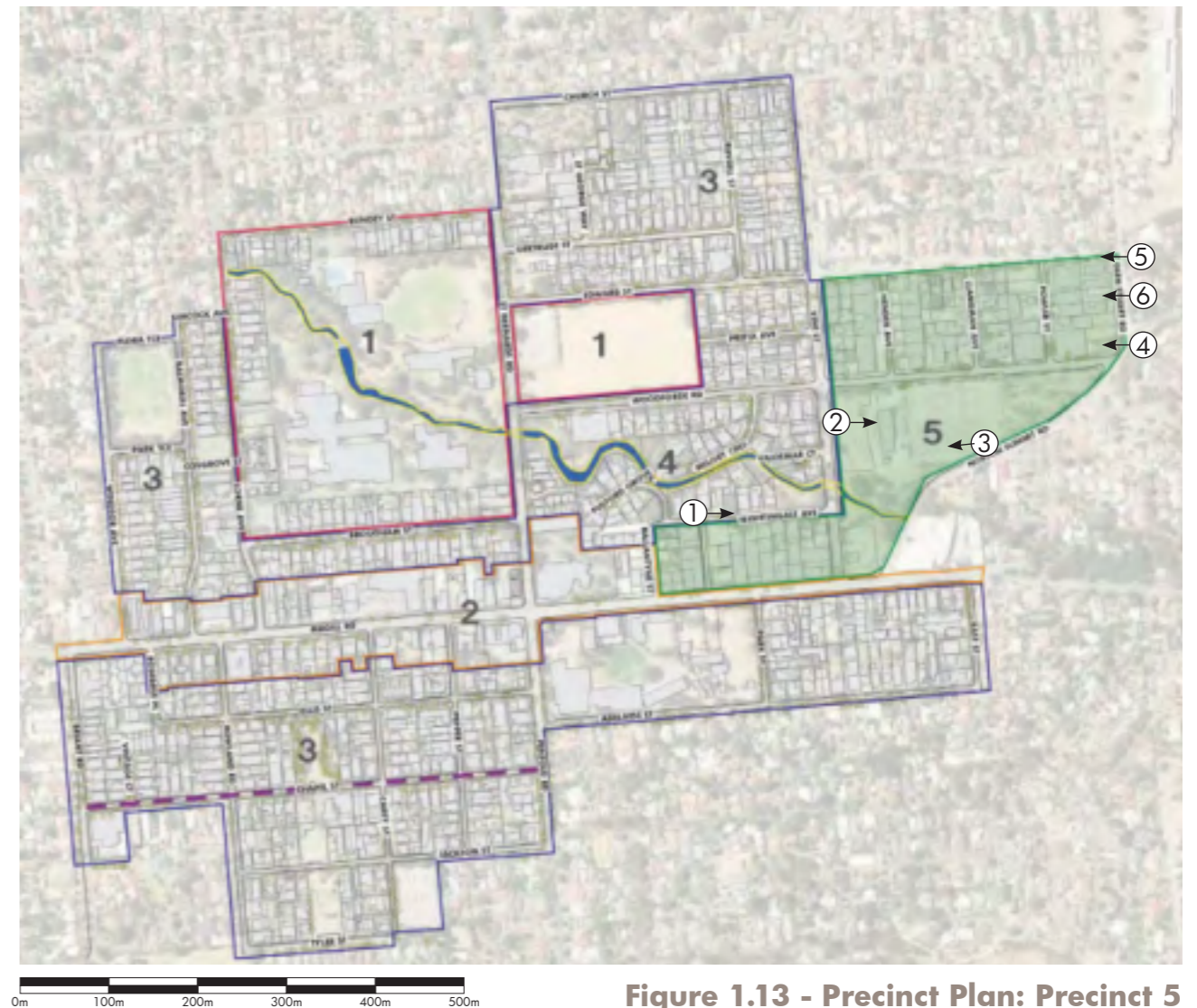


Figure 1.13 - Precinct Plan: Precinct 5



Figure 1.14 - Precinct 5 (Industrial Precinct)

### 1.3 BACKGROUND DOCUMENTS

The following documents have been reviewed in the preparation of this report.

- Burnside (City) Development plan, Consolidated - 28 February 2013



- Campbelltown Council Development Plan, Consolidated - 28 February 2013



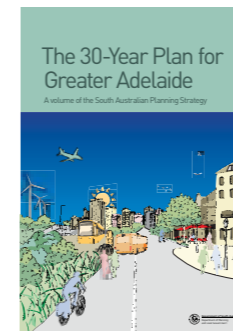
- An Urban Design Protocol for Australian Cities - Creating Place for People, Australian Government - November 2011



- Our Cities, Our Future - A National Urban Policy for a Productive, Sustainable and Liveable Future, Australian Government - May 2011



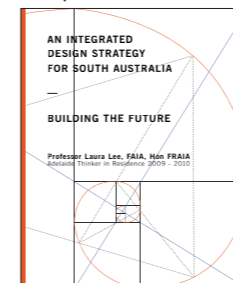
- The 30-Year Plan for Greater Adelaide (30-year Plan), Government of South Australia: Department of Planning Transport Infrastructure - 2010



- Future Directions: Optimising Our Transport Corridors, Government of South Australia: Department of Planning Transport Infrastructure - August 2012



- An Integrated Design Strategy for South Australia - Building the Future, Professor Laura Lee, Adelaide Thinkers in Residence - 2010



## 1.4 PURPOSE OF THE REPORT AND PROJECT OBJECTIVES

This report is a design based report and identifies a strategy for the renewal of the Magill Village and includes a masterplan and urban design framework for the study area.

Currently the study area is economically depleted with short term and/or low grade industrial businesses interspersed throughout run down residential areas. Therefore, the key project objective is to transform the study area into a vibrant, economically viable destination that supports a number of creative industries and commercial activities in a high quality urban environment. The overall success of the project will depend largely on the economic success of the initiatives implemented. As such, there is also a commercial basis for the Masterplan recommendations. A number of redevelopment sites are identified and implementation strategies have been proposed.

The following objectives were formed from consultation with Campbelltown City Council and the City of Burnside:

1. Improvements to public space (including road treatments, trees, vegetation, pavements, stormwater/Water Sensitive Urban Design and the like) to improve the overall character and experience of the precinct. This includes Magill Road commercial precinct, industrial precinct, education precinct and typical suburban streets. Improved physical connections within the precinct
2. Movement - planning for all transit modes, seeking to improve the efficiency and experience around car parking, pedestrian accessibility, cycling access and the like
3. Adaptive reuse options for the current Magill University of South Australia campus, exploring models of future development. These can include options for supporting a creative community and associated creative business/industry and education incubators
4. Enhance the recreational assets of the precinct (especially Third Creek and the existing reserves) and consider how they can better contribute to the life and vitality of the village community
5. Develop a clear brand and identity for the precinct that communicates the unique offerings of the Village (products, history and social qualities) to reinforce an authentic experience
6. Identify an implementation strategy with cost estimates. In delivering the project, promote and encourage collaboration amongst stakeholders, education sector partners, local and state government agencies and expert industry body representatives
7. Document the delivery process and findings, to serve as a national demonstrator for best-practice design
8. Build a higher quality urban environment
9. Improve regional accessibility to attract business and investment
10. Rejuvenate retail activity in the Village
11. Attract passing trade to stop and use the Village
12. Promote accessibility, improve pedestrian connections.



## 1.5 ACKNOWLEDGEMENTS

### 1.5.1 DELIVERY PARTNERS

The following is a list of members within the Magill Village Partnership delivery team.

#### **Campbelltown City Council**

Paul Di iulio (Campbelltown City Council CEO)  
Kevin Lowe (General Manager, Urban Planning & Leisure Services)  
Michelle Hammond (General Manager, Corporate & Community Services)  
Andrian Wiguna (General Manager, Infrastructure Services)  
Jodie Terp (Senior Planner Urban Policy)  
Nigel Litchfield (Manager Planning Policy & Environment/Sustainability)  
Ursula Hickey (Manager, Economic Development & Leisure Services)  
Lyn Townsend (Manager, Governance & Strategic Planning)  
Lois Dunkerley (Community Engagement Officer)  
Andrew King (Contract and Fleet Coordinator)

#### **City of Burnside**

Paul Deb (City of Burnside CEO)  
Graeme Brown (General Manager, Urban Services)  
Mark Buckerfield (Manager, Asset Services)  
Magnus Heinrich (Manager Development Services)  
Nigel Morris (General Manager, Corporate Services)  
Maggie Wood (Manager, Community Engagement)

#### **COMPLETE Urban Pty Ltd**

Scott Williams (Director)  
Bob Matchett (Director)  
David Spencer (Principal of Architecture)  
Jason Pruszinski (Manager, South Australia)  
Hamish McLauchlan (Manager, Development)  
Nathan Parish (Traffic Engineer)  
Josephine Gooderham (Civil Designer)  
Lee Andrews (Manager, HUB)  
Jai O'Sullivan (Architect)  
Alice Hawker (Urban Designer)  
Rhys Vincent (Graduate of Architecture / Urban Designer)

#### **Donovan & Associates**

Peter Donovan (Director)

#### **Elevation Design**

Luke Dickson (Director)

#### **Gehl Architects**

David Sim (Director)  
Henriette Vamberg (Director)

#### **University of South Australia**

Jane Andrew (Director, Matchstudio)  
Jane Lawrence (Program Director, Interior Architecture)  
Stephen Ward (Program Director, Architecture)

#### **WT Partnership**

Tony Brewster (Director)  
Luke Crilley (Associate)



## 1.5.2 STAKEHOLDER PARTNERS

The following is a list of stakeholders who have contributed to the Magill Village Partnership project delivery.

### **Adelaide Hills Council**

Andrew Aitken (Adelaide Hills CEO)  
Marc Salver (Director of Planning & Development Services)

### **City of Prospect**

Justin Commons (Director of Corporate Services and Major Projects)

### **Department for Education and Child Development**

Ross Treadwell (Director, Capital)  
Vince Dimasi (Assistant Director, Asset Services)

### **Department of Planning, Transport and Infrastructure: Planning**

John Hanlon (Deputy Chief Executive)  
Andrew McKeegan (Executive Director, Planning Reform & Projects: Planning Division)  
Michael Davis (Principal Planner)

### **Department of Planning, Transport and Infrastructure: Transport Services Division**

Andy Milazzo (Deputy Chief Executive; Transport Services Division)  
Paul Gelston (Director, Road & Traffic Management)  
Andrew Excell (Manager, Metro Region – Traffic Management)  
Paul Bennett (Acting Manager Strategic Traffic Planning)  
Phil Lawes (Manager Strategic Traffic Planning)  
Denise Hatzi (Project Manager - Metro Division)

### **Donovan & Associates**

Peter Donovan (Director)

### **Elevation Design**

Luke Dickson (Director)

### **Magill Businesses**

Bennett's Pottery  
Romeo's Foodland  
Institute Building  
Magill Road Shopping Complex, Magill

### **RenewalSA**

Fred Hansen (Renewal SA CEO)  
Richard McLachlan (General Manager, Riverbank, Infrastructure & Project Coordination)

### **University of South Australia**

Philip Clatworthy (Director, Facilities Management)  
Dr Delene Weber (Program Director, Barbora Hardy Institute)  
Prof. Steffen Lehmann (Professor of Sustainable Design)  
Prof. Chris Daniels (Director, Barbora Hardy Institute)



## 2.0 Context



# 2.0 Context

## 2.1 HISTORY

As part of this Masterplan report, COMPLETE engaged Donavon and Associates, History and Historic Preservation Consultants to undertake a historical assessment of the precinct. A copy of their detailed report is contained in the Appendix to this report. However, the key findings from the report are summarised as follows:

1838	<ul style="list-style-type: none"> <li>Village established when Magill Estate was laid out by Robert Cook and William Ferguson on the 80 acre Section 285. Land was offered for sale on 20 October 1838.</li> <li>Village named after Sir David Maitland Mackgill.</li> </ul>
≈1838	<ul style="list-style-type: none"> <li>Orchards were established in the sections through which Third Creek flowed.</li> </ul>
1840	<ul style="list-style-type: none"> <li>Subdivision of Section 342 occurred soon after Section 285 and included a quarry in the eastern portion of the estate supplying stone for near by houses. It was subdivided in 1854.</li> </ul>
1844	<ul style="list-style-type: none"> <li>World's End Inn established.</li> </ul>
1845	<ul style="list-style-type: none"> <li>The Auldana Vineyard was planted.</li> </ul>
1846	<ul style="list-style-type: none"> <li>School established in current Pepper Street Art Centre.</li> </ul>
1848	<ul style="list-style-type: none"> <li>St Anglican Church of St George was built.</li> </ul>
1853 - 1856	<ul style="list-style-type: none"> <li>Magill Village came under separate local Government Administration (East Torrens in June 1853 and Burnside in August 1856).</li> </ul>
1854	<ul style="list-style-type: none"> <li>East Torrens Hotel established (renamed Tower Hotel in 1910).</li> </ul>
1855	<ul style="list-style-type: none"> <li>Methodist Church built.</li> </ul>
1856	<ul style="list-style-type: none"> <li>Section 343 was subdivided into larger blocks closer to the Adelaide Hills and smaller blocks nearer to the Magill Village.</li> </ul>
1860's - 1870's	<ul style="list-style-type: none"> <li>Business development concentrated the intersection of Magill Road, St Bernard's and Penfold Roads and initially comprised the East Torrens Hotel, a grocer's store and a butcher.</li> </ul>
1866	<ul style="list-style-type: none"> <li>Baptist Church, Magill Road built.</li> </ul>
1882	<ul style="list-style-type: none"> <li>Post Office established.</li> <li>Tram established with terminus at Penfold Road. Extended to Norton Summit Road in 1912 and removed in 1958.</li> </ul>

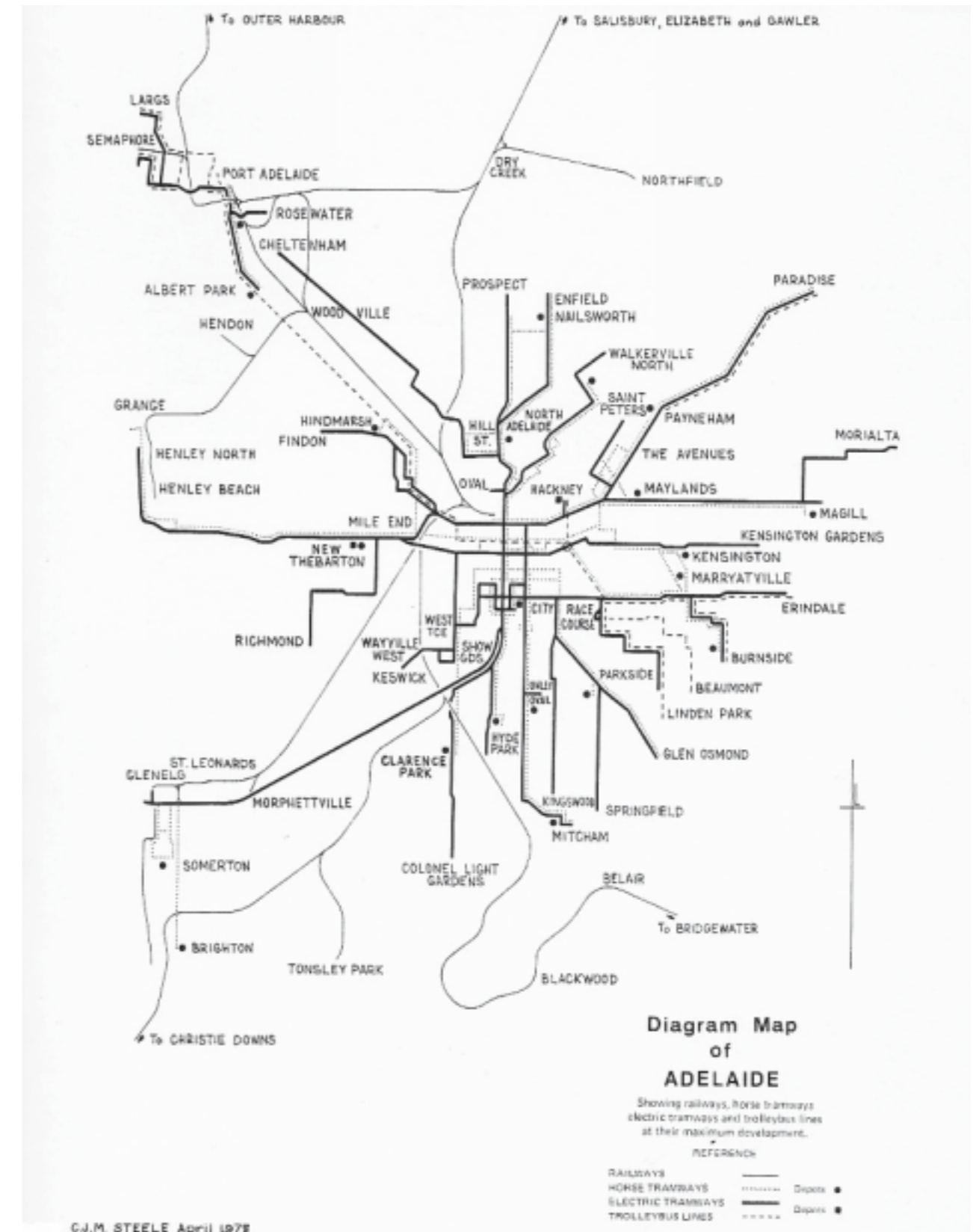


Figure 2.1 - Transport Plan of Adelaide (1878 - 1978)

## 2.1 HISTORY Cont.

1887	• Bennett Magill Pottery established.
Early 1900's	• Residential subdivision into smaller blocks.
1901	• Institute Building built on Magill Road.
1909	• Police Station established in current real estate building (closed 1976).
1919	• Magill War Memorial unveiled (relocated to current site in 1956).
1923	• Magill Grain Store established on Magill Road.
1926	• Primary School established on Penfold Road.
1959	• ETSA sub-station built.
2002	• Vine Street Rose Garden completed.

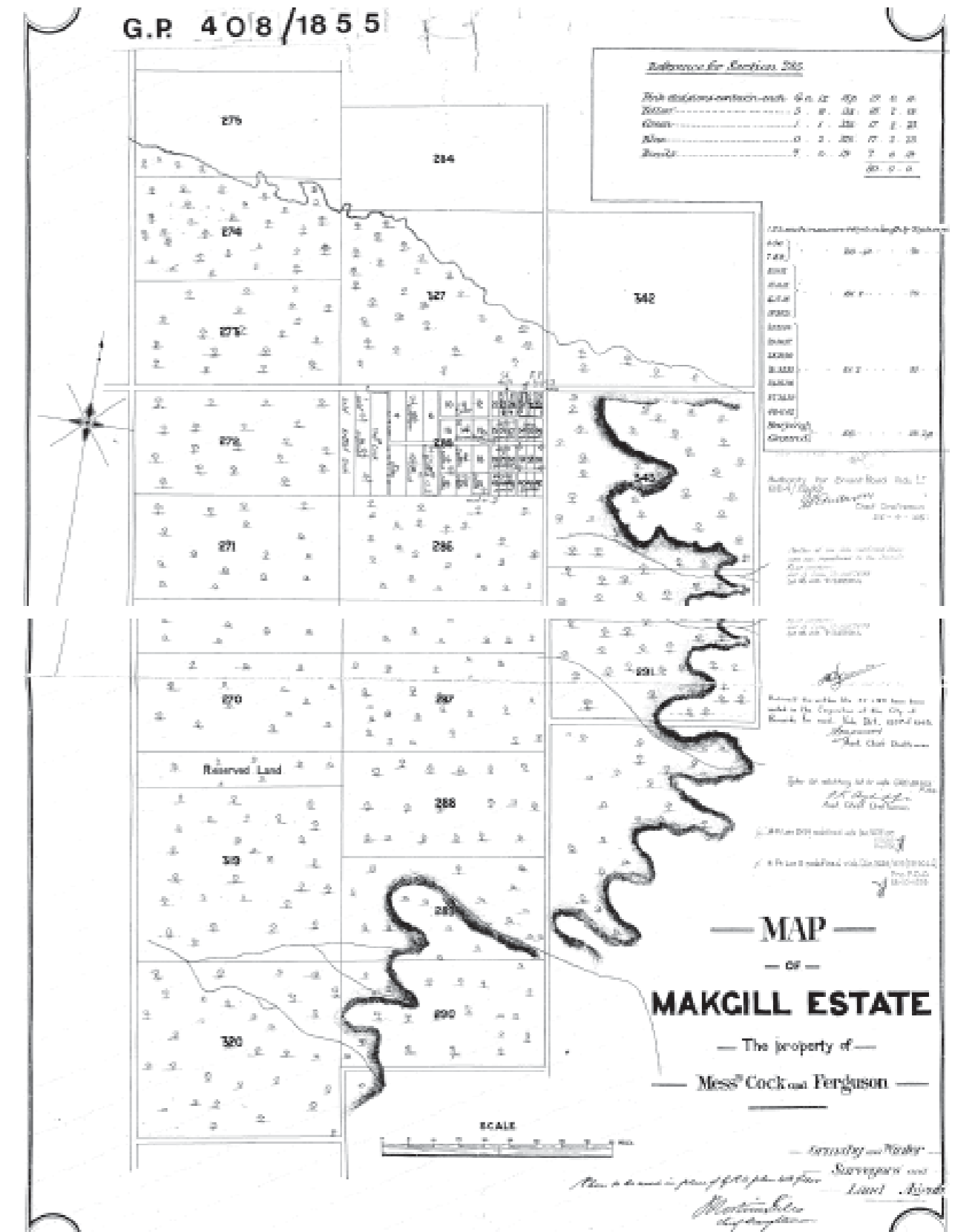


Figure 2.2 - Historic Background (First Subdivision forming Magill Village - 1838)

## 2.1 HERITAGE

There are several properties within the study area that are assessed as being of heritage value. Several of these are public and institutional buildings of social and cultural value that help to define the character of the Precinct. The majority of places are residential properties which reinforce the essential residential nature of the village.

Woodforde House, 10 Melory Crescent, Magill, in the Campbelltown local government area is the only property included on the State Heritage Register.

Local heritage properties in the Campbelltown local government area are:

- 30 Bunday Street, Magill – dwelling;
- 23 Central Avenue, Magill – dwelling;
- 48 Central Avenue, Magill – dwelling;
- 50 Central Avenue, Magill – dwelling;
- Chandler Court, Magill – King George Hall;
- 12 Church Street, Magill – dwelling;
- 9/8 Holmes Court, Magill – dwelling;
- 24 Jervois Avenue, Magill – dwelling;
- 7 Lorne Avenue, Magill – dwelling;
- 12 Lorne Avenue, Magill – dwelling;
- 607 Magill Road, Magill – former police station;
- 609-611 Magill Road, Magill – Institute;
- 613 Magill Road, Magill – former post office;
- 615 Magill Road, Magill – shops;
- 621 Magill Road, Magill – Tower Hotel;
- 68 Shakespeare Avenue, Magill – dwelling
- 14-38 St Bernards Road, Magill – Murray House;
- 45 St Bernards Road, Magill – St George’s Anglican cemetery;
- 45 St Bernards Road, Magill – St George’s Church
- 6 Windsor Avenue, Magill – dwelling

Local heritage properties in the Burnside local government area are:

- 28 Briant Road, Magill – Bennett’s Magill Pottery;
- 26 Chapel Street, Magill – Morialta Uniting Church;
- 18 Ellis Street, Magill – dwelling;
- 20 Ellis Street, Magill – dwelling;
- 18 Jackson Street, Magill – Cemetery and trees;
- 496 Magill Road – Bible Presbyterian Church;
- 558 Magill Road, Magill – former primary school;
- 572 Magill Road, Magill – Soldiers’ Memorial;
- 574 Magill Road, Magill – grain store;
- 6 Penfold Road, Magill – dwelling;
- 7 Penfold Road, Magill – dwelling;
- 13 Penfold Road, Magill – dwelling;
- 38 Penfold Street, Magill – Former Seaview Champagne Cellars
- 2 Pepper Street, Magill – dwelling;
- 4 Pepper Street, Magill – dwelling;
- 6 Pepper Street, Magill – dwelling;
- 10 Pepper Street, Magill – dwelling;
- 11 Pepper Street, Magill – dwelling;
- 24 Romalo Avenue, Magill – dwelling;
- 1 Rosedale Place, Magill – dwelling;
- 553 The Parade, Magill – dwelling.

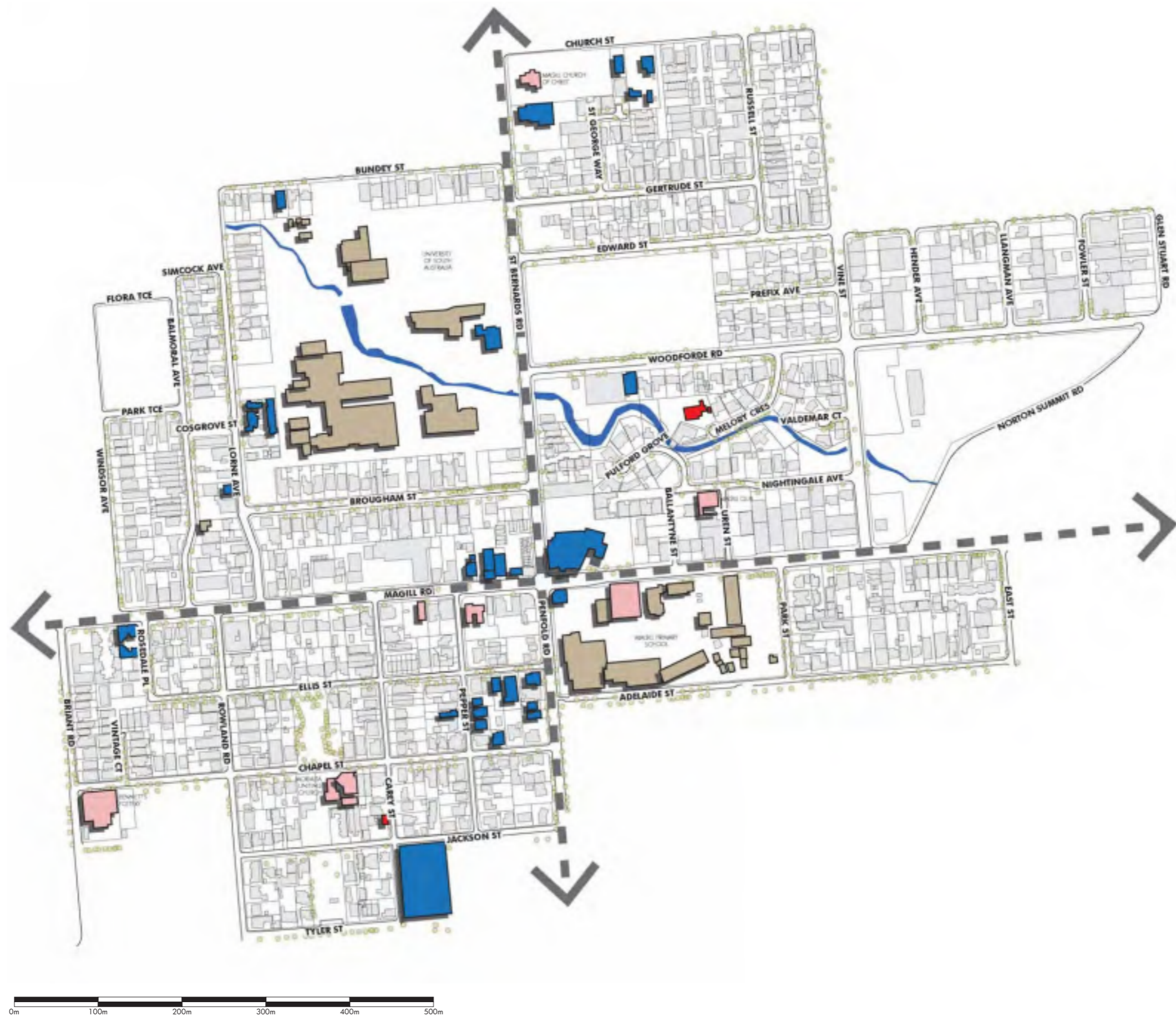


Figure 2.3 - Heritage





## 3.0 Study Area Analysis



## 3.0 Study Area Analysis

---

### 3.1 STUDY AREA ANALYSIS

The study area is a highly urbanised environment with a wide range of land uses existing in close proximity to each other. It is well serviced by an extensive traffic network that heavily favours cars over other forms of transport. Penfold/St Bernards Roads are a major north-south traffic route east of the city.

Land use is predominantly residential with pockets of education, light industrial and commercial located throughout. This diverse mix of use is ad hoc in nature, reduces the quality of the residential areas and does not contribute to the overall amenity of the study area. There are areas of green open space scattered through the precinct, although these are not readily accessible from major traffic routes and therefore they are not well used. For example, Third Creek is a significant tract of open space land that is either part of the University site or largely on land located behind residential areas.

Car parking is centred around the commercial areas on Magill Road and the University site. Parking on Magill Road tends to be located in separate areas behind the buildings leading to numerous carpark entry points along Magill Road. Magill Road is a relatively wide road due to its former use as a major tram route east of the CBD. Currently the excessive road width is poorly defined and is dominated by vehicular traffic. There are areas where footpath widths are narrow with significant crossfall and do not support pedestrian movement. Cycling infrastructure is poor along Magill Road and more generally within the precinct.

As a result of the relatively small block sizes the majority of the residential areas are dense with a large built area to open space ratio, particularly in the area south-east of the Magill Road and Penfold/St Bernards Roads intersection. The residential areas to the north of Magill Road are generally post war development with numerous more recent medium density infill residential projects. These have been introduced throughout the residential area and their random nature has further eroded the original residential character.

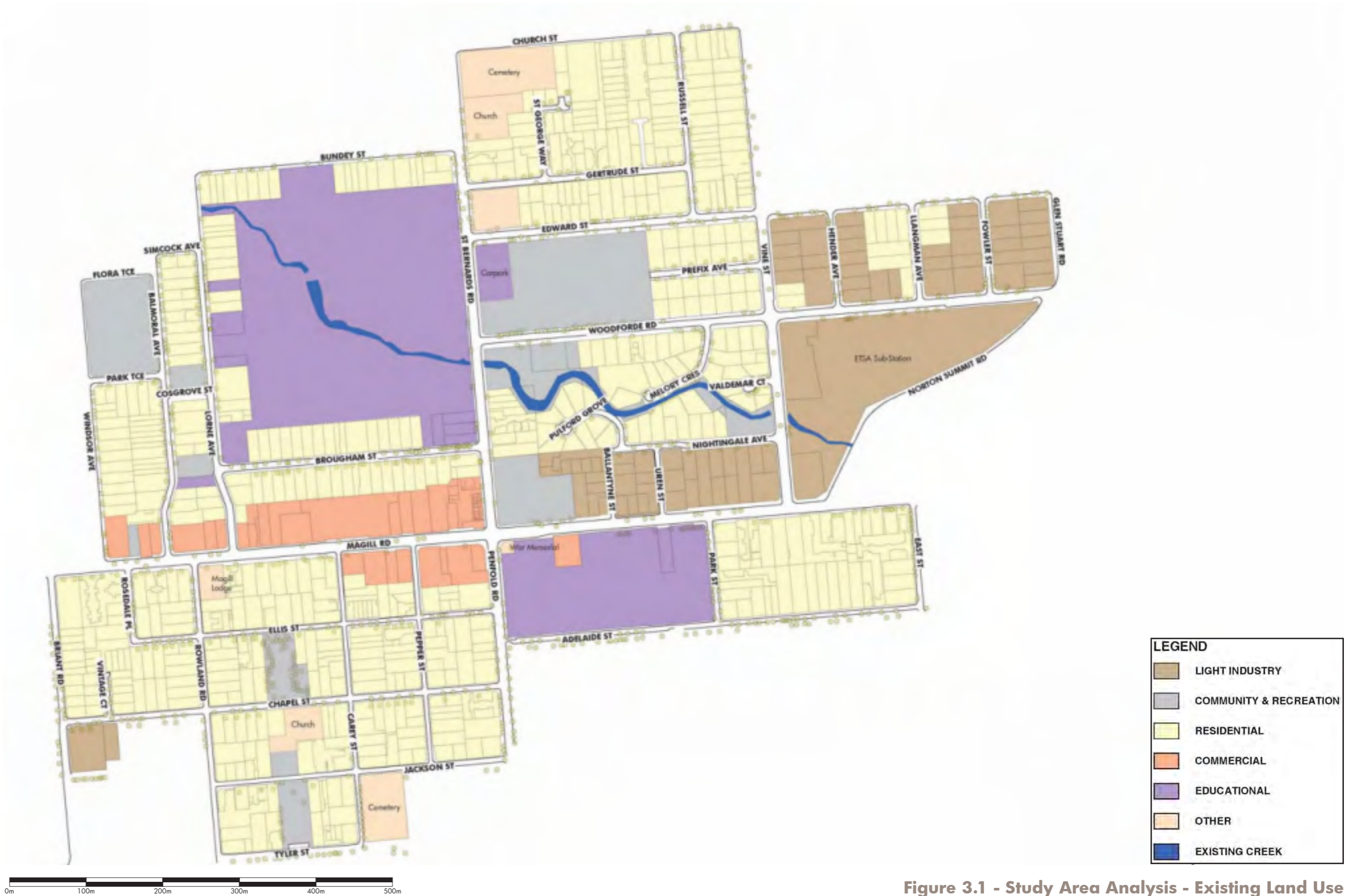


Figure 3.1 - Study Area Analysis - Existing Land Use

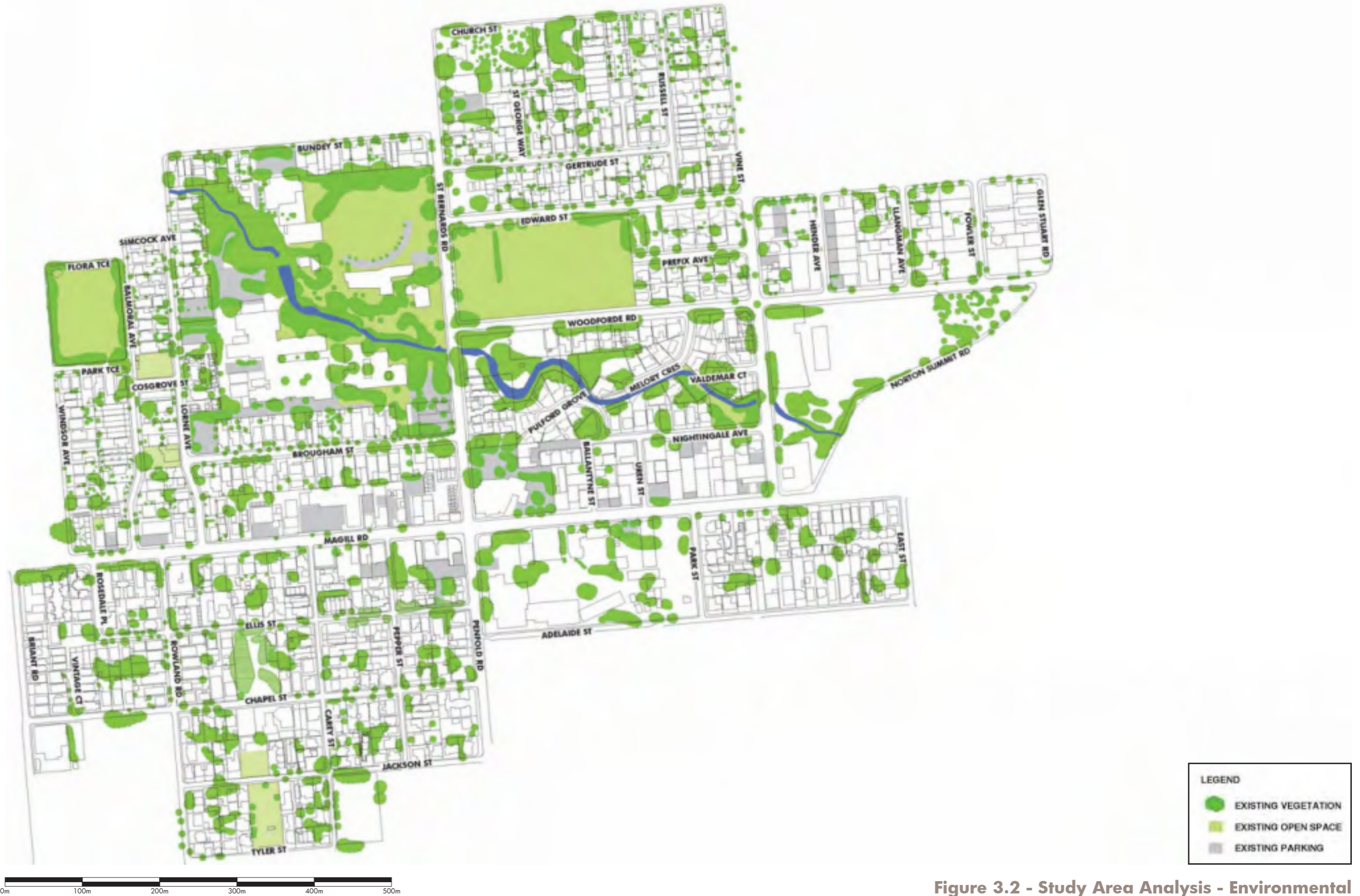


Figure 3.2 - Study Area Analysis - Environmental



**LEGEND**  
 ■ EXISTING BUILDINGS

**Figure 3.3 - Study Area Analysis - Built Form**



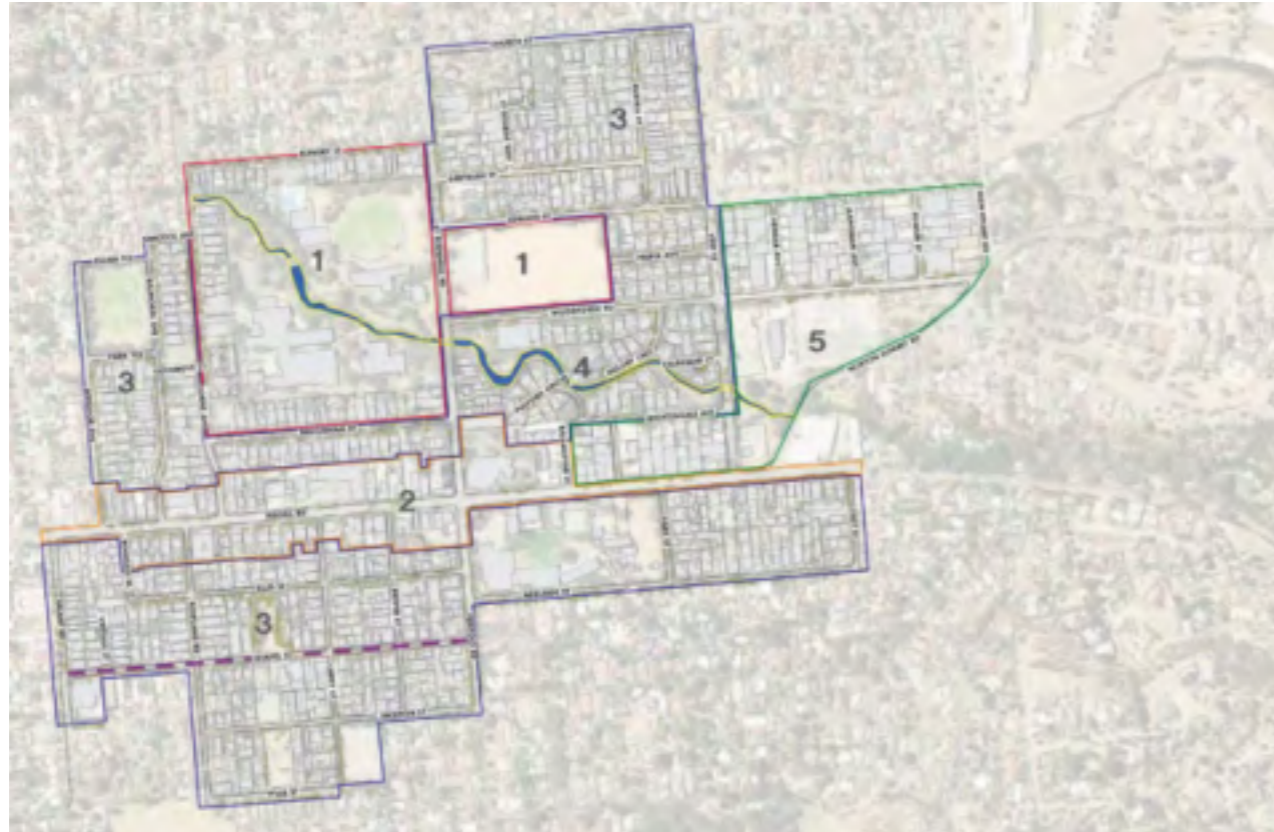
## 4.0 Constraints and Opportunities



# 4.0 Constraints and Opportunities

## 4.1 CONSTRAINTS AND OPPORTUNITIES

In order to identify the desired future character of the study area, assessment of the existing constraints and potential opportunities has been undertaken for each of the 5 precincts.



### Precinct 1 - Education Precinct - University of South Australia: Magill Campus

The University of South Australia site will become a key site within the study area over the next few years as the planned relocation of the campus to the Adelaide CBD occurs. The constraints of the existing site and the constraints presented by relocation are:

- Disconnected from Magill Road;
- Located within a residential area limiting future uses;
- Site bisected by Third Creek; and
- Extensive purpose built building stock;
- Recent low quality purpose built student housing presents a poor streetscape character and may have less demand for ongoing use.

The potential opportunities of the existing site:

- Connect University Precinct to Magill Road Precinct;
- Establish distinct zones within the University Precinct to capitalise on existing natural amenity or existing building stock;
- Greater connections to surrounding residential streets;
- Activate Third Creek within Precinct and make it a central part of passive and active recreation opportunities and the central focus for the village; and

- Introduce commercial activities consistent with the 30-Year Plan for both St Bernard's Road and within the precinct.

### Precinct 2 - Commercial Precinct - Magill Road

The Magill Road Precinct is on the boundary of two Council's and is traffic dominated. Its main constraints are:

- High volume, high speed traffic environment;
- Poor cycling amenity;
- Lack of connections, links, accessibility to wider precinct areas;
- Poor built form and overall appearance of buildings with a wide range of incompatible uses;
- Poor quality street furniture;
- Lack of public open space/meeting space/ outdoor dining areas
- Steep terrain contributing to pedestrian accessibility issues;
- Lack of greenery, street trees, shade;
- Lack of play spaces;
- Lack of places to sit;
- No coherent trader group to market and promote the centre to the wider community;
- Retail is dispersed and fragmented;
- Lack of identity;
- Existing day-to-day retail is not attracting passing trade;
- Inadequate and uncoordinated parking;
- Irregular signage; and
- Poor pedestrian amenity with narrow footpath and poor paving quality.

The Magill Road Precinct has the following main opportunities:

- Convert the inactive edges to non retail uses and consolidate retail uses to the core;
- Potential for integrated community facility to be developed and create civic precinct around Pepper Street Art Studio;
- Upgrade Magill Road streetscape to make more pedestrian friendly and to reduce road width and to encourage other modes of transport;
- Consolidate and integrate car park to reduce cross overs to street and reduce overall car park footprint;
- Redevelop supermarket shopping centre to improve streetscape character with majority of carparks placed underground;
- Connect east end of Magill Road to Third Creek to encourage pedestrian activity;
- Connect Magill Road Precinct to University Precinct
- Create village square commercial centre off Magill Road;
- Introduce 40 km/hr off-peak zone between Pepper Street and St Bernards/Penfold Roads;
- Upgrade footpaths to activate street frontage including lighting, street furniture, landscape and paving; and
- Provide greater exposure for commercial businesses through Magill Village marketing and heightened brand awareness.



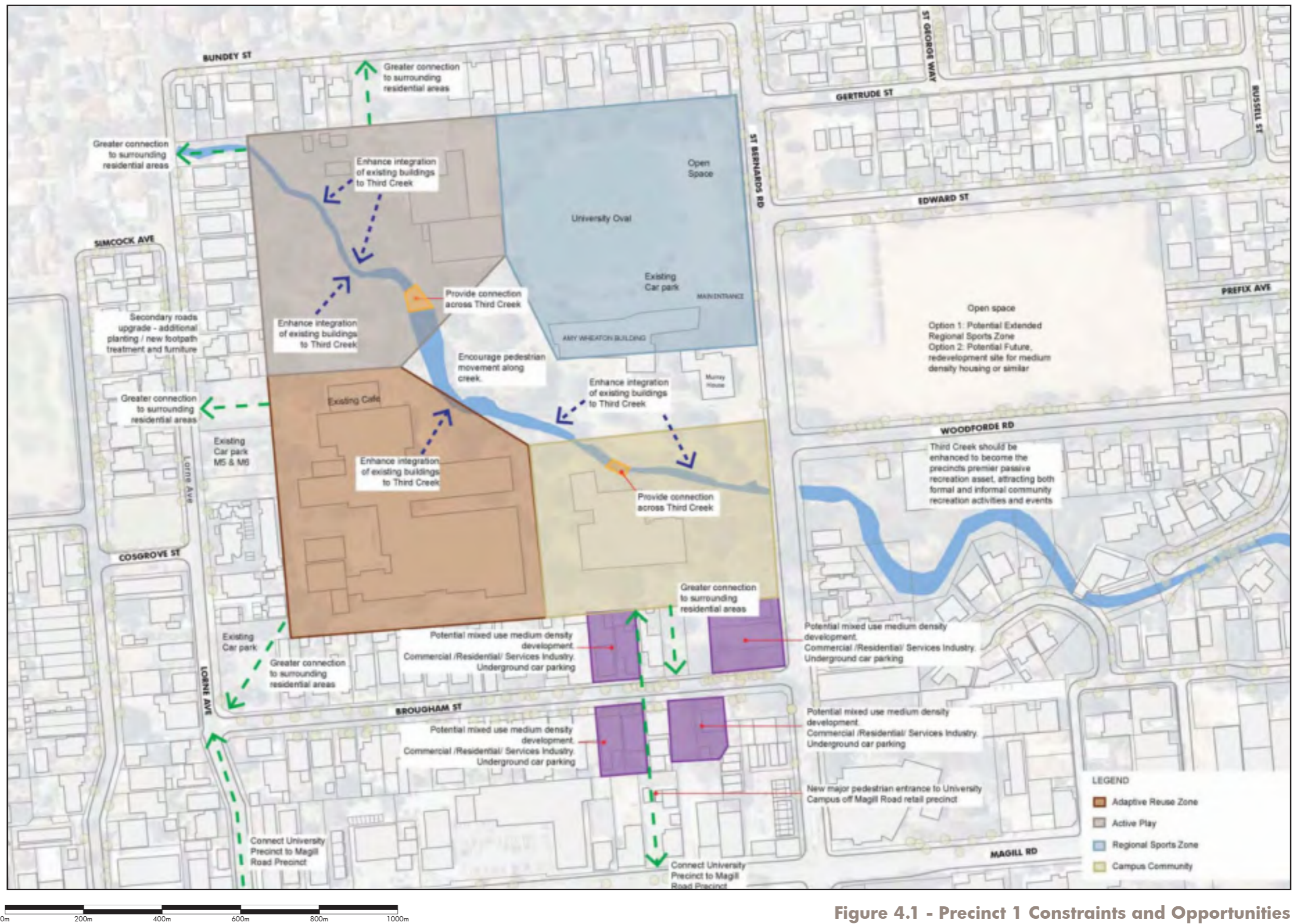
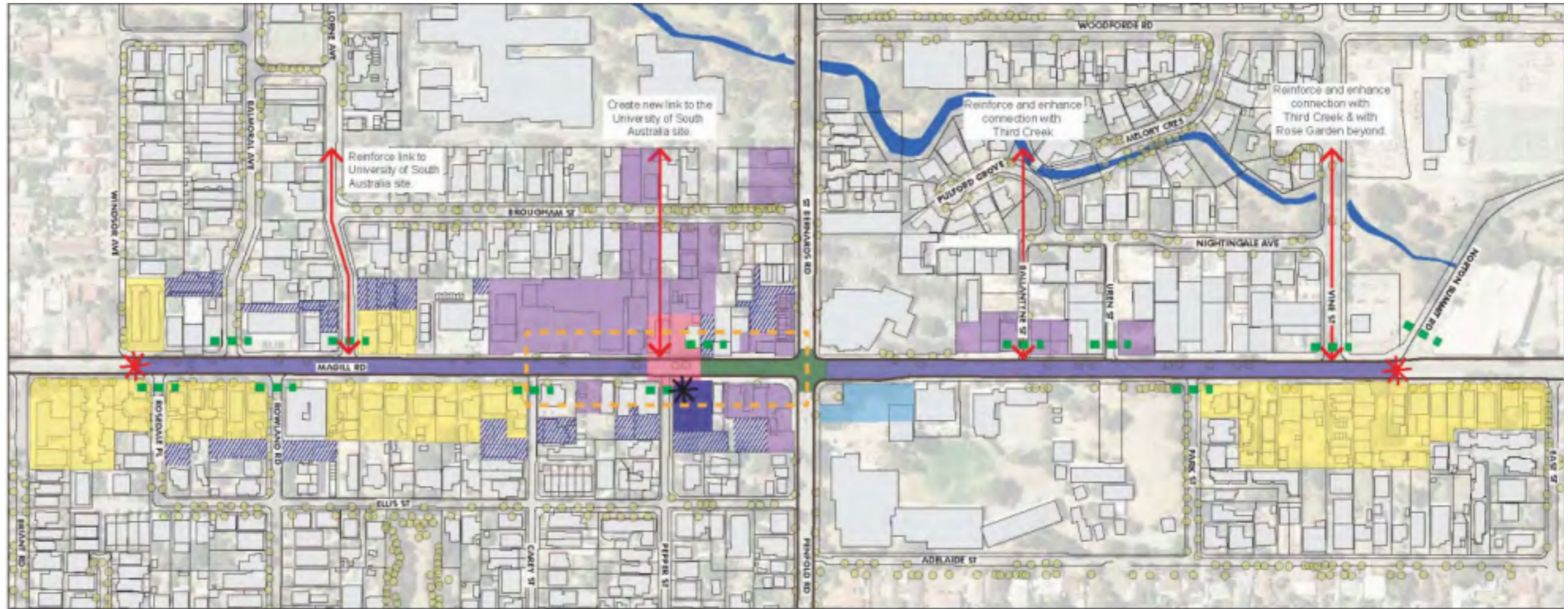


Figure 4.1 - Precinct 1 Constraints and Opportunities



- Streetscape Upgrade  
Enhance pedestrian access within the Magill Village by providing a comfortable environment, including street trees and quality landscaping, comfortable street furniture, safe and level pavement surfaces. Implement reduced road space to encourage slower traffic speeds and higher pedestrian amenity as well as a continuous bicycle lane.
- Commercial development opportunity.  
Redevelopment of the existing Magill Road Supermarket and speciality shops should seek to build to the street boundary, to reinforce a more consistent village character. New street level facades should be interesting and visually engage with pedestrians.
- Feature treatment to create sense of arrival/new threshold to signify slower road speed zone.
- Create vibrant streetscape to reinforce the Magill Primary School identity.
- Raised pavement crossings on all side streets in retail environments and any other location where pedestrian safety is an issue.
- Proposed Village Square  
Reactivate Pepper Street and Pepper Street Art Gallery by providing:  
- accessible, wider footpaths;  
- public art;  
- connection with the University;  
- new landscaping;  
- safer pedestrian crossings; and  
- seating and gathering areas
- Create variable speed zone (40-60km/hr).  
New landscaping to emphasise local area of cultural interest. Slow traffic with signage and raised carriageway. Remove existing kerb & gutters. New furniture, bike racks and appropriate outdoor dining. Rationalise traffic lanes at Magill Road and St Bernards/Penfold Roads intersection.
- Existing residential properties on Magill Road.
- A vibrant village heart with a range of interesting & eclectic commercial, social, cultural & civic activities. New feature lighting, planting and furniture to reinforce gateway village feel.
- Focal point location for possible art installation/ interpretive signage.
- Carparking off Magill Road should be consolidated and integrated seeking to reduce the high number of driveways off Magill Road interrupting pedestrian movement.
- Connect Magill Road with Third Creek, through Balantyne Street, Vine St and new pedestrian link opposite Pepper Street. Enhance connections with new paving/ interpretive signage and new planting.



**Figure 4.2 - Precinct 2 Constraints and Opportunities**

### Precinct 3 - Typical Residential Precinct - using Chapel Street as an example

There is a wide range of differing pedestrian streets in the study area. They generally favour the car over pedestrians and provide a low level of amenity. The main issues with the current residential streets are:

- Lack of greenery and shade;
- Lack of activation, which could contribute to CPTED issues;
- Road surfacing is large in contrast with narrow footpaths;
- Lack of ownership or sense of community by residents;
- Excessive crossfall with poorly maintained paving materials;
- Lack of unity and identity;
- Lack of places to sit; and
- Lack of cycling infrastructure.

The opportunities existing to upgrade the residential streets in order to improve their amenity. Chapel Street could be used as a pilot project to demonstrate the following improvements:

- Upgrade paving to reduce poor pedestrian amenity and improve accessibility;
- Improve street tree planting to provide more shade;
- Celebrate cultural history of the precinct by introducing cultural/heritage walk of key features; and
- Improve pavement crossing to enhance pedestrian experience.



#### **Precinct 4 - Third Creek**

Third Creek runs diagonally across the study area and crosses through a number of diverse land use areas. A walking trail has been established along the creek route. However, it is highly disjointed due to St Bernards Road and land use east of St Bernards Road. Consequently it is not well patronised and presents potential CPTED issues.

The key issues are:

- Proximity to St Bernards Road;
- Stairs - cyclists and wheelchair users are unable to use parts of the trails;
- Insufficient creek crossovers and creek crossovers in some areas are not wide enough to accommodate two way pedestrian movement;
- Private properties, electrical substations and main roads disconnect the trail and limit public access;
- Moderate safety issues due to uneven terrain and traffic along St Bernards Road;
- Lack of signage indicating trail route entrance;
- No proper surfacing;
- Insufficient width; and
- Potential CPTED issues.

Third Creek is a valuable asset within the region and a revitalised creek setting will contribute to the success of the village. The following opportunities will help realise this vision:

- Provide greater accessibility to creek from all surrounding precincts;
- Third Creek to become educational exemplar for Adelaide's south-east Creeks to bring heightened awareness of Adelaide's arid climate and water conservation imperatives;
- Strengthen connection of Third creek to the recreation area south of Woodforde Road; Celebrate the natural setting of the creek within its surroundings, particularly in the University Precinct; and
- Seek to bring the landscape qualities of Third Creek to be more accessible off Magill Road, both visually and physically.

### **Precinct 5 - Industrial Precinct**

The industrial precinct east of St Bernards/Penfold Road is disconnected from the remainder of the study area by roads and by Third Creek. The existing industrial uses do not encourage collaborative interaction with other users within the precinct, particularly education uses and commercial activities, where creative synergies should be explored and fostered.

The Industrial Precinct has the following opportunities:

- Provide greater exposure for industrial businesses through Magill Village marketing and heightened brand awareness.
- Foster and encourage those industries that support and reinforce the vibrant multicultural history of the area particularly ethnic net based manufacturing industries, potteries, terrazzo concrete industries. Encourage creative interaction with local education facilities.



## 5.0 Vision and Guiding Principles



# 5.0 Vision and Guiding Principles

## 5.1 BACKGROUND TO VISION AND GUIDING PRINCIPLES

The following Magill Village vision and guiding principles, emerged out of a number of research and background activities that collectively inform this Masterplan.

The main contributors to the Vision and Guiding Principles were:

- Detailed study area audit and photographic survey of each of the precincts;
- Desktop analysis of existing literature, including historical research;
- Review of existing land use, zoning and heritage;
- Constraint and opportunities analysis; and
- November 2012 “Building Better Cities” event coordinated by COMPLETE. Invited participants included government, local business, the education sector and professional association representatives. The event comprised a series of workshops led by Gehl Architects from Copenhagen. Gehl Architects are considered one of the world’s leading urban quality experts. At the community workshop they spoke about world’s best practice in building resilient communities that are attractive and desirable to nurturing creative class, sharing best practice approaches that could be applied in Magill. The stakeholder workshop considered in detail the opportunities for Magill Village from a global best-practice lens. Discussions were then held with the Project Team in walking the Village.

The Draft Vision and Guiding Principles were put to all stakeholders and the broader precinct community through a joint Council formal consultation process. All feedback from this consultation process is included in the Appendix of this report. After responding to all feedback and input, the Vision and Guiding Principles were formally endorsed by both the Campbelltown City Council and the City of Burnside Council. The endorsed Vision and Guiding Principles are included on the following page and these form the aspirational brief for the development of the Masterplan.

An illustrated version of the Guiding Principals is also included in the Appendix to this report.







# Magill Village Partnership Vision

Magill Village will support the local community’s everyday recreation, social, shopping, education and business needs; offering to all who visit, an authentic, appealing and memorable experience.

## Guiding Principles

### 1. IMPROVE IMAGE

#### 1.1 Develop an Authentic Village Identity

Magill Village should have a unique personality and identity, authentic in its reflection of local life – a celebration of the precinct’s culture, history and context.

#### 1.2 Enhance Village Character

Magill Village should be a welcoming and attractive place that encourages people of all ages to visit and stay. Key public and recreation spaces should be softened by an abundance of trees and vegetation, providing an intimate character that compliments the natural character of Third Creek.

#### 1.3 Reinforce the Village Centre

The Magill Road shopping precinct should become more recognizable as the village heart – a vibrant destination that supports the daily business, shopping, education and social needs of the local community.

#### 1.4 Coordinate Signage

A coordinated approach for Magill Road signage (advertising, wayfinding and interpretive) should be developed, seeking to reduce visual clutter and support a more consistent and recognizable precinct identity.

### 2. ENHANCE CONNECTIONS

#### 2.1 Promote the Unique History

Magill Village holds strong associations with people and ways of life, both past and present. This rich history should be preserved and creatively communicated to reinforce an authentic village identity, celebrating the people and their achievements that make the place unique.

#### 2.2 Link Key Attractions

Magill Village offers a broad range of services to its community, including a growing café/restaurant precinct, local wineries, active and passive recreation, arts & culture, education (early learning, primary, secondary & tertiary), light industrial, goods manufacturing, professional service industries, retail businesses and the like. Awareness of the many key attractions within the precinct should be improved, strongly linking the presently disparate parts.

#### 2.3 Connect with Third Creek

Third Creek provides a relaxing public space, inviting all ages to enjoy its natural setting. Third Creek should be:

- Physically and visually connected to Magill Road shopping precinct;
- Integrated as a core element of the Magill Village identity;
- Enhanced to become the precinct’s premier passive recreation asset, attracting both formal and informal community recreation activities and events; and
- More accessible to the general community, particularly the section of Third Creek within the existing University site.

### 3. IMPROVE MOBILITY

#### 3.1 Improve Cycling Experience

In support of State Government transport planning objectives, future upgrades to Magill Road should see it become the major eastern suburbs cycling route (commuter and recreation), connecting Adelaide City to Adelaide Hills, with Magill Village offering a destination stop. Future design of Magill Road should afford a high priority to cyclists, providing separation of cyclists from moving traffic as part of a pleasant and safe cycling environment.

#### 3.2 Improve Walking Experience

Pedestrian access to and within Magill Village, especially the Magill Road shopping precinct, should be enhanced by providing a comfortable environment, including shade, street trees and quality landscaping, comfortable street furniture, safe and level pavement surfaces.

#### 3.3 Implement Road Dieting

In support of an improved Magill Road shopping experience, vehicle traffic should be slowed and pedestrian access between both sides of Magill Road improved, including crossing wait times shortened. Any excess road carriageway width should be “slimmed” in support of more equitable needs for pedestrians and cyclists, especially within the Magill Road shopping precinct.

#### 3.4 Consolidate Car Parking

Rear car parking off Magill Road shopping precinct should be consolidated and integrated wherever possible, seeking to reduce the high number of driveways presently interrupting pedestrian movement. New developments should seek to underground car parking, avoiding at-grade car parking where possible.

### 4. BUILD ECONOMY

#### 4.1 Focus Economic Development

Build critical mass by locating and supporting key community services in the Village heart.

Seek ways to support, attract and retain unique “destination” businesses that will reinforce the village identity, especially around creative/artistic industry, specialist food, organic products and the like.

#### 4.2 Build Partnerships

All businesses in the village should recognise their role in reinforcing the Magill Village identity, in support of economic growth. Consider initiatives that will foster strong partnerships between businesses. Link with broader region initiatives such as the Campbelltown Food Trail and consider parallel new initiatives such as a Heritage Trail and/or Arts Trail.

#### 4.3 Support a Village for Staying

Support a village that is desirable for staying, developing a more active café and arts culture concentrated around the Magill Road shopping precinct and Third Creek. Ensure base infrastructure is provided (e.g. public toilets, public seating, drinking water, interactive children’s play space).

### 5. MAXIMISE REDEVELOPMENT POTENTIAL

#### 5.1 Maximise Existing Sporting Infrastructure

The sporting based recreation facilities associated with the existing University site, should wherever possible, be used increasingly by the community and a more direct physical and visual connection made with Third Creek - the premier recreation asset for the village and wider region.

#### 5.2 Attract “Creative Class”

A strong emphasis on attracting the “creative-class” should be fostered in all future Magill Village redevelopment initiatives, reinforcing the existing creative businesses in the precinct. Notably the existing University site represents a key catalyst site in support of future urban growth and development for Magill Village. Future redevelopment of the University site will optimally see a tertiary education facility continue in some form, supplemented with broader mixed-use development.

#### 5.3 Support Supermarket Site Redevelopment

Redevelopment of the existing Magill Road supermarket and specialty shops should seek to build to the street boundary, reinforcing a more consistent main street village character. New street level facades should be interesting and visually engage with pedestrians.

Figure 5.1 - Vision and Guiding Principles

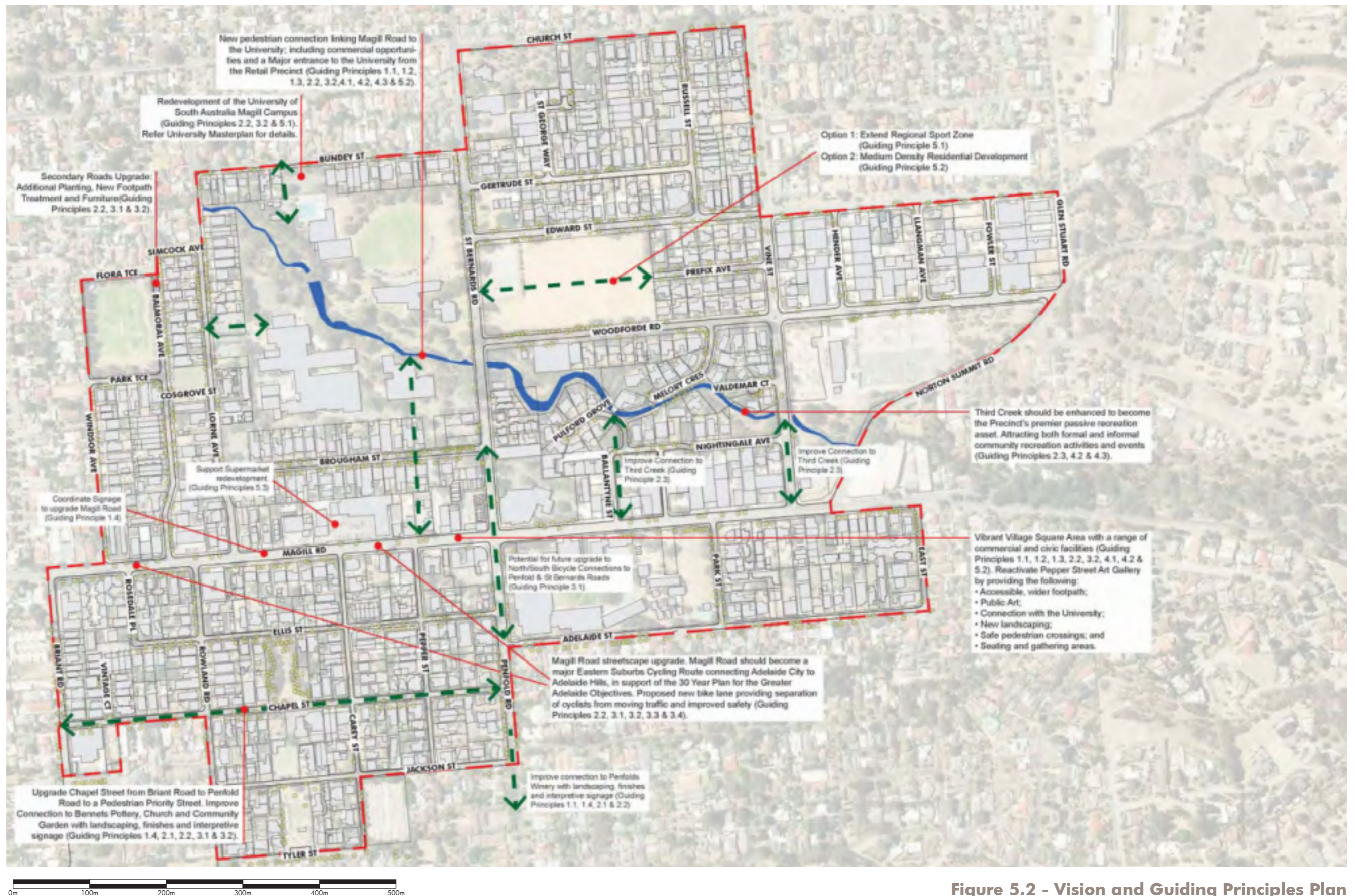


Figure 5.2 - Vision and Guiding Principles Plan

## 6.0 Masterplan



# 6.0 Masterplan

## 6.1 OVERALL MASTERPLAN

Magill Road along with the University of South Australia site are the two most significant components of this Masterplan and together these two precincts will be the catalyst for a revitalised Magill Village that achieves the objectives of the 30-Year plan and the respective strategic plans for both Councils. A detailed Masterplan has been prepared for these two precincts showing how the agreed project objectives can be implemented into the current urban setting. In broad terms the Masterplan proposes:

### **Create a streetscape that supports vibrant street life**

An upgraded Magill Road with new paving, seating, lighting and landscaping will revitalise the area and improve pedestrian amenity. This coupled with new public art, interpretative signage and increased outdoor dining areas will contribute significantly to the realisation of the Magill Village and set a precedent for future improvements within the precinct and beyond. Upgrade of the public spaces will also provide a catalyst for private investment and economic renewal.

### **Provide a unified streetscape that is high quality, functional, robust and timeless in design**

An improved streetscape that places a higher priority on pedestrians and alternative modes of transport will reduce the overwhelming influence of the car, supporting a reduction in car parking areas and making the area safer and more vibrant.

### **Integrate trees and landscape treatments to provide shade, unify streetscape and provide interest**

The Masterplan proposes a simple landscape palette that will unify all areas and create a unique character that provides plenty of shade and makes the Village a pleasant place.

### **Integrate water sensitive Urban Design (WSUD) into the streetscape to treat stormwater runoff**

It is important that surface water run-off be dealt with before it enters the stormwater system. Planting areas have been proposed along Magill Road at strategic locations to capture stormwater to nourish proposed new trees and plants.

### **Create green corridors with increased tree planting and soft landscape**

The creation of green corridors particularly between Magill Road and Third Creek will encourage pedestrian movement between the two areas and create the foundation for future commercial opportunities in line with the 30-Year Plan for Greater Adelaide.

### **Encouraging and designing for pedestrian, cycling and transit use**

It is proposed to reduce the road carriageway width of Magill Road within the study area whilst still accommodating the existing one lane of traffic in each direction. Additional space gained will be used to create wider footpaths, additional street trees and plantings and a dedicated and uninterrupted bike lane. Street parking will be rationalised in a combination of on street and indent parking interspersed with additional landscaping. This will result in a more cyclist and pedestrian friendly area. Magill Road speed limit, particularly between Pepper Street and St Bernards/Penfolds Road will be reduced to 40 km/hr.

### **Improve existing bicycle network to facilitate safe, accessible connections to desirable destinations**

The proposed bike lanes should be connected to the broader bikeway network for Adelaide. The Department of Planning, Transport and Infrastructure has nominated Magill Road as the Major (Regional) Cycling Route from City to eastern suburbs and the Adelaide Hills.

### **Provide amenity such as bike racks, drinking fountains and seats to support bicycle and pedestrian use**

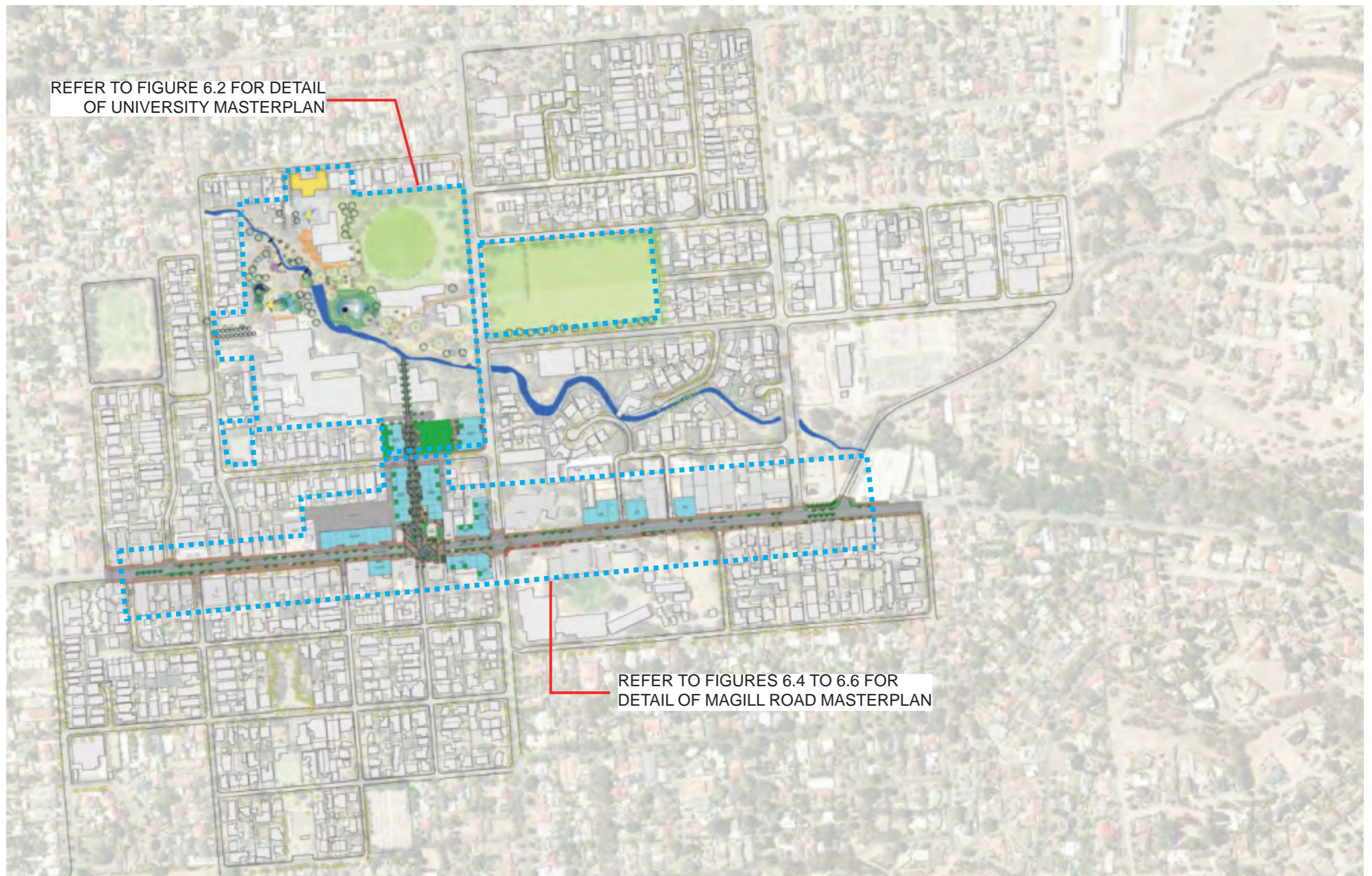
Amenities for pedestrians and bike users should be provided to encourage these users to stay in the precinct. The Village can provide a vital cyclist node between City and Hills and this function would support a strengthened cafe industry for the Village.

### **Ensure accessible design in accordance with Australian Standards**

Upgrade to footpaths throughout the study area should be accessible including crossovers to private properties, footpath widths and cross falls and raised pedestrian crossings.

### **Materials and planting palette shall express the sense of place and local identity**

Paving materials, street furniture and other public related elements should be robust. It is proposed to use natural materials with earthy tones to celebrate and promote the local industries such as potteries, concrete works, and to reinforce the natural character of Third Creek.



REFER TO FIGURE 6.2 FOR DETAIL OF UNIVERSITY MASTERPLAN

REFER TO FIGURES 6.4 TO 6.6 FOR DETAIL OF MAGILL ROAD MASTERPLAN



Figure 6.1 - Overall Masterplan

- (A) Active play / sports zone  
Proposed active recreational & educational zone (10 - 15 year olds)  
- Bmx Track  
- Skate Park  
- Climbing Wall  
- Exercise Stations
- (B) Proposed adventure play / education space (5 - 10 year olds)
- (C) Proposed early learning play education open space / playground (1 - 5 year olds)
- (E) Connection to street  
Adaptive re-use zone  
- Education transition  
- Research  
- Business incubator  
- Creative industry hub
- (F) Formal campus community space connected with Murray house (outdoor cinema, markets, events etc)  
Adaptive re-use zone  
- Conference facility  
- Hotel  
- Education  
- Research
- (J) Potential sports / education community related building  
Option 1: Extended Regional sports zone  
Option 2: Future University redevelopment site for medium density housing or similar
- (K) Future pedestrian boulevard (under option 2)
- (M) Bridge connection over Third Creek
- (N) Demolish existing building
- (P) Proposed new wetland and outdoor educational facility
- (Q) Modify existing sports buildings to have visual outlook onto Third Creek

**LEGEND**










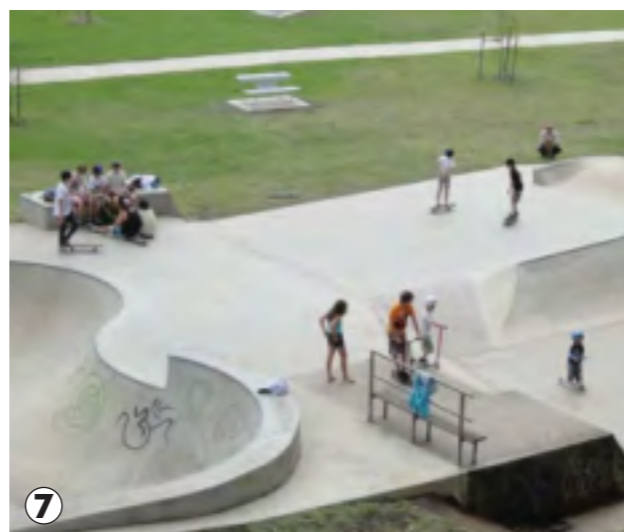
-  Proposed shared path / Pedestrian path
-  Proposed new trees (*Fraxinus angustifolia* - Claret Ash Tree & *Corymbia ficifolia* - Red Flower Gum)
-  Proposed BBQ / Picnic shelter
-  Focal point for public art / interpretive signage
-  Proposed exercise station
-  New timber deck
-  Existing buildings
-  Existing road / Carpark
-  Existing trees



Figure 6.2 - University Masterplan



**LEGEND**

1. Adventure play with challenge and risk
2. Wetland and nature education
3. Creek and interactive play with public space
4. Play spaces catering for diversity of age
5. Creek exploration
6. Formal spaces with natural character
7. Youth active recreation
8. Pedestrian connections over water as play space
9. Medium density development

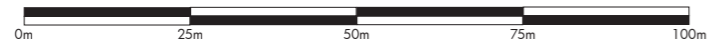
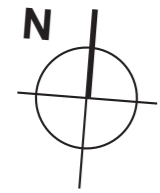
**Figure 6.3 - Education Precinct (Reference Images)**



**LEGEND**

- Extent of upgrade
- Proposed new trees (*Fraxinus angustifolia* - Claret Ash Tree & *Corymbia ficifolia* - Red Flower Gum).
- Proposed mixed use commercial/residential opportunities
- ✳ Focal point for public art
- New garden beds. May act as a bio-retention filter to improve stormwater runoff. Also provides a more pedestrian scale and focus to the streetscape**
- Proposed bike lane (1.2m wide)
- Existing kerb to be removed
- Footpath - Existing footpath widened where possible. New paving

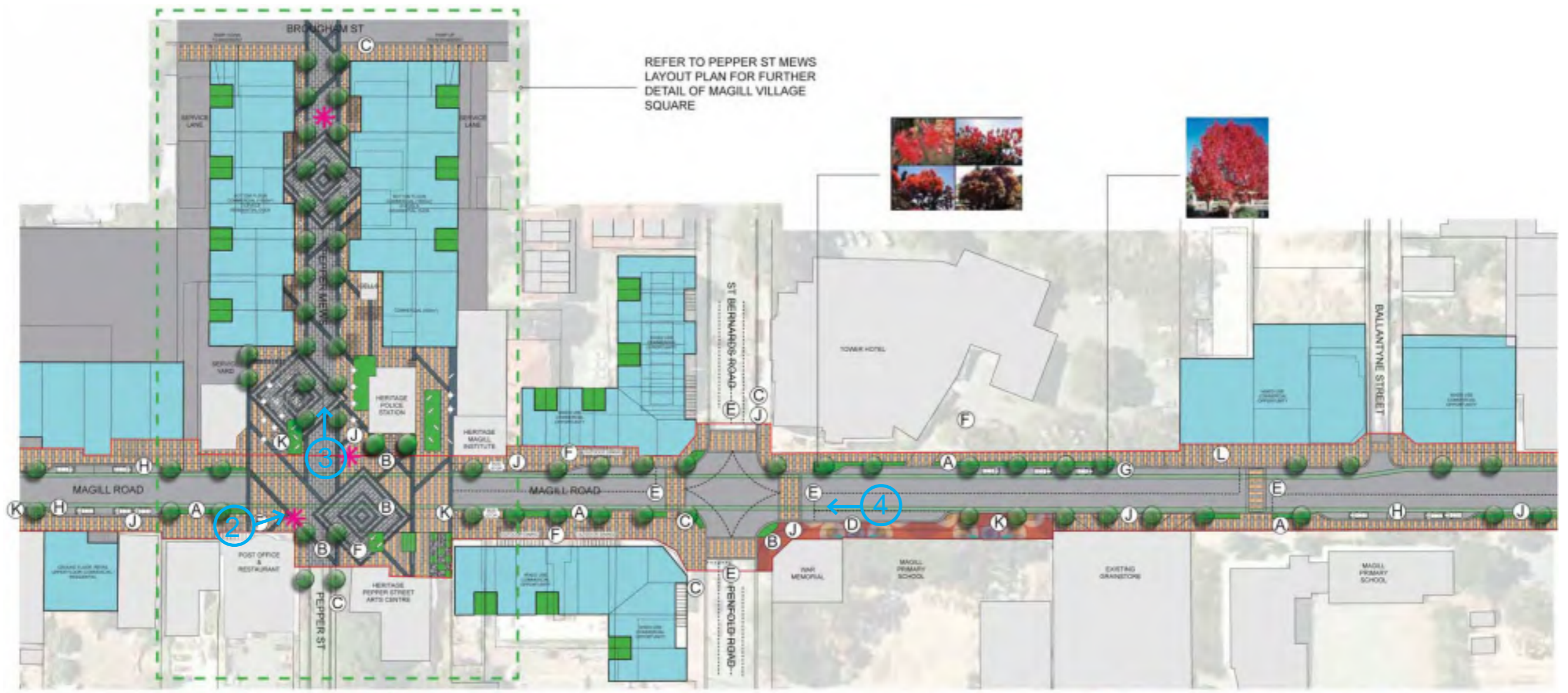
- (A)** New public seating integrated with raised planter beds
- (B)** Feature uplighting
- (C)** Possible location for interpretive element. Example indicating Penfolds Winery & Third Creek
- (D)** Feature colour concrete paving to reinforce the identity of Magill Primary School
- (E)** Coloured raised paving pedestrian crossing. Reduce signalled wait times for pedestrians where applicable
- (F)** Outdoor dining opportunity
- (G)** New mixed use commercial building. Redevelopment should seek to build to the street boundary to reinforce a more consistent village character
- (H)** Indent parking
- (J)** Cultural & Heritage walk marker (QR Code)
- (K)** Rubbish bin
- (L)** Footpaths designed to have suitable surface materials for all users. Increased footpath width to provide for disability access, space for gathering, seating and/or the provision of outdoor dining
- (M)** Reduce carriageway to one lane where appropriate to reduce traffic speed and allow for a widened footpath
- (N)** New planted median. Possible rain garden to frame Magill Village entry



○ PHOTOMONTAGE LOCATIONS

**Figure 6.4 - Magill Masterplan 1/3**





REFER TO PEPPER ST MEWS LAYOUT PLAN FOR FURTHER DETAIL OF MAGILL VILLAGE SQUARE



**LEGEND**

- Extent of upgrade
- Proposed new trees (*Fraxinus angustifolia* - Claret Ash Tree & *Corymbia ficifolia* - Red Flower Gum).
- Proposed mixed use commercial/residential opportunities
- \* Focal point for public art
- New garden beds. May act as a bio-retention filter to improve stormwater runoff. Also provides a more pedestrian scale and focus to the streetscape
- Proposed bike lane (1.2m wide)
- Existing kerb to be removed
- Footpath - Existing footpath widened where possible. New paving

- (A)** New public seating integrated with raised planter beds
- (B)** Feature uplighting
- (C)** Possible location for interpretive element. Example indicating Penfolds Winery & Third Creek
- (D)** Feature colour concrete paving to reinforce the identity of Magill Primary School
- (E)** Coloured raised paving pedestrian crossing. Reduce signalled wait times for pedestrians where applicable
- (F)** Outdoor dining opportunity
- (G)** On street parking
- (H)** Indent parking
- (J)** Cultural & Heritage walk marker (QR Code)
- (K)** Rubbish bin
- (L)** Footpaths designed to have suitable surface materials for all users. Increased footpath width to provide for disability access, space for gathering, seating and/or the provision of outdoor dining



PHOTOMONTAGE LOCATIONS

Figure 6.5 - Magill Masterplan 2/3



**LEGEND**

- Extent of upgrade
- Proposed new trees (*Fraxinus angustifolia* - Claret Ash Tree & *Corymbia ficifolia* - Red Flower Gum).
- Proposed mixed use commercial/residential opportunities
- Focal point for public art
- New garden beds. May act as a bio-retention filter to improve stormwater runoff. Also provides a more pedestrian scale and focus to the streetscape
- Proposed bike lane (1.2m wide)
- Existing kerb to be removed
- Footpath - Existing footpath widened where possible. New paving

- (A)** New public seating integrated with raised planter beds
- (B)** Feature uplighting
- (C)** Possible location for interpretive element. Example indicating Penfolds Winery & Third Creek
- (D)** Feature colour concrete paving to reinforce the identity of Magill Primary School
- (E)** Coloured raised paving pedestrian crossing. Reduce signalised wait times for pedestrians where applicable
- (F)** Outdoor dining opportunity
- (G)** On street parking
- (H)** Indent parking
- (J)** Cultural & Heritage walk marker (QR Code)
- (K)** Rubbish bin
- (L)** Footpaths designed to have suitable surface materials for all users. Increased footpath width to provide for disability access, space for gathering, seating and/or the provision of outdoor dining
- (M)** Reduce carriageway to one lane where appropriate to reduce traffic speed and allow for a widened footpath
- (N)** New planted median. Possible rain garden to frame Magill Village entry

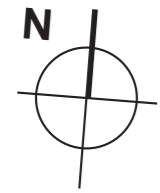


Figure 6.6 - Magill Masterplan 3/3



**LEGEND**

- Extent of upgrade
- Proposed new trees (*Fraxinus angustifolia* - Claret Ash Tree & *Corymbia ficifolia* - Red Flower Gum).
- Proposed mixed use commercial/residential opportunities
- Focal point for public art
- New garden beds. May act as a bio-retention filter to improve stormwater runoff. Also provides a more pedestrian scale and focus to the streetscape
- Proposed bike lane (1.2m wide)
- Existing kerb to be removed
- Footpath - Existing footpath widened where possible. New paving

- A** New public seating integrated with raised planter beds
- B** Feature uplighting
- C** Possible location for interpretive element. Example indicating Penfolds Winery & Third Creek
- D** Signalised crossing moved to define Village square
- E** Coloured raised paving pedestrian crossing. Reduce signalised wait times for pedestrians where applicable
- F** Outdoor dining opportunity
- G** Bollards to align with corridor of trees
- H** Indent parking
- J** Cultural & Heritage walk marker (QR Code)
- K** Rubbish bin
- L** Footpaths designed to have suitable surface materials for all users. Increased footpath width to provide for disability access, space for gathering, seating and/or the provision of outdoor dining
- M** Greenery introduced with outdoor dining to provide more intimate spaces within the square
- N** Outdoor seating to introduce elements of public art within the square and streetscape
- O** New trees within the hardscape are aligned to create a more formal and pedestrian friendly space
- P** Opportunities for pop up restaurants/ coffee shops activating blank walls

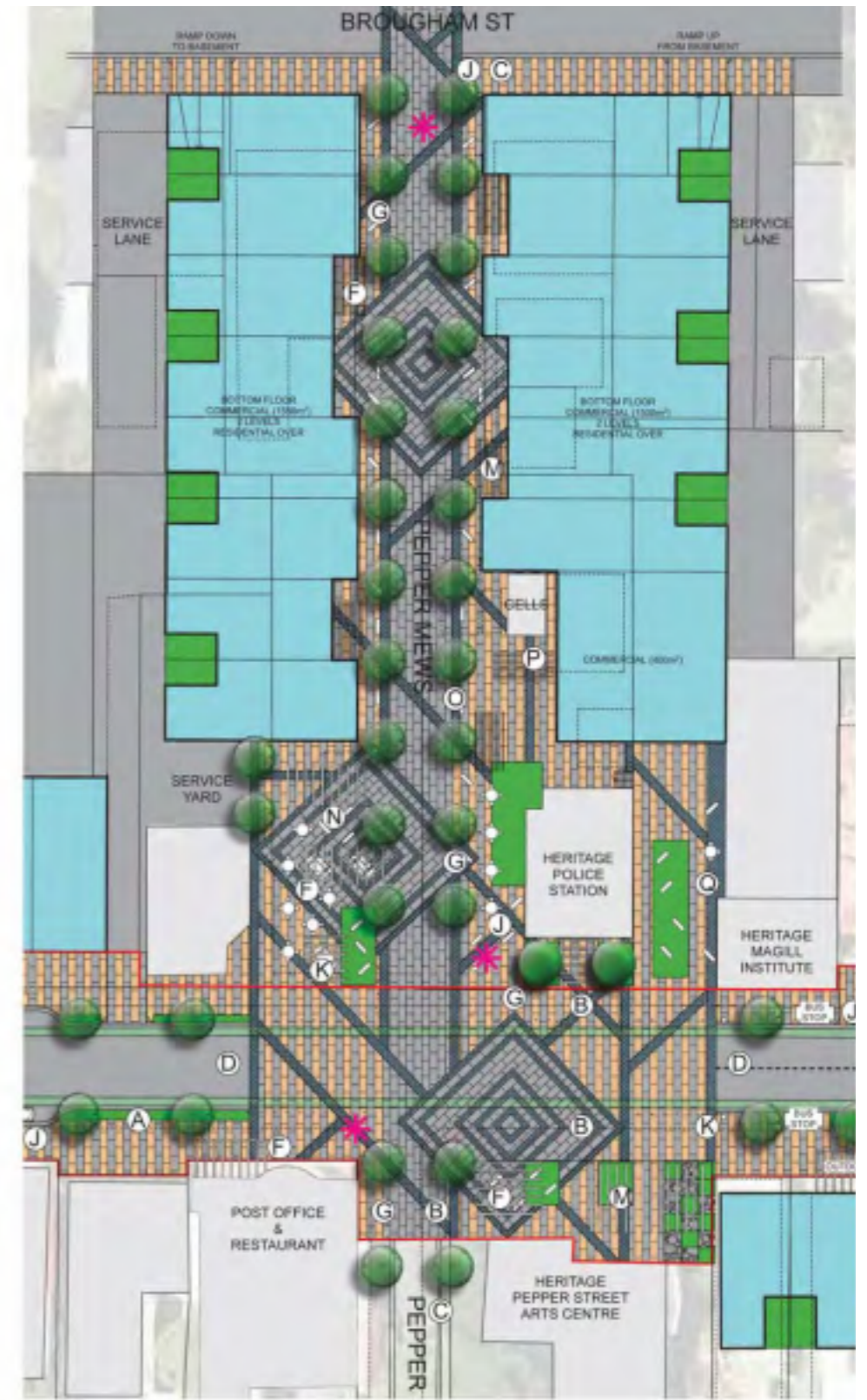


Figure 6.7 - Pepper Mews Layout Plan 1/2



**LEGEND**

- Extent of upgrade
- Proposed new trees (*Fraxinus angustifolia* - Claret Ash Tree & *Corymbia ficifolia* - Red Flower Gum).
- Proposed mixed use commercial/residential opportunities
- ✳ Focal point for public art
- New garden beds. May act as a bio-retention filter to improve stormwater runoff. Also provides a more pedestrian scale and focus to the streetscape
- Proposed bike lane (1.2m wide)
- Existing kerb to be removed
- Footpath - Existing footpath widened where possible. New paving

- (A)** New public seating integrated with raised planter beds
- (B)** Feature uplighting
- (C)** Possible location for interpretive element. Example indicating Penfolds Winery & Third Creek
- (D)** Signalised crossing moved to define Village square
- (E)** Coloured raised paving pedestrian crossing. Reduce signalised wait times for pedestrians where applicable
- (F)** Outdoor dining opportunity
- (G)** Bollards to align with corridor of trees
- (H)** Indent parking
- (J)** Cultural & Heritage walk marker (QR Code)
- (K)** Rubbish bin
- (L)** Footpaths designed to have suitable surface materials for all users. Increased footpath width to provide for disability access, space for gathering, seating and/or the provision of outdoor dining
- (R)** Redefined Uni access road

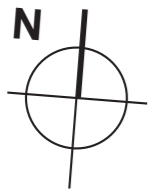


Figure 6.8 - Pepper Mews Layout Plan 2/2

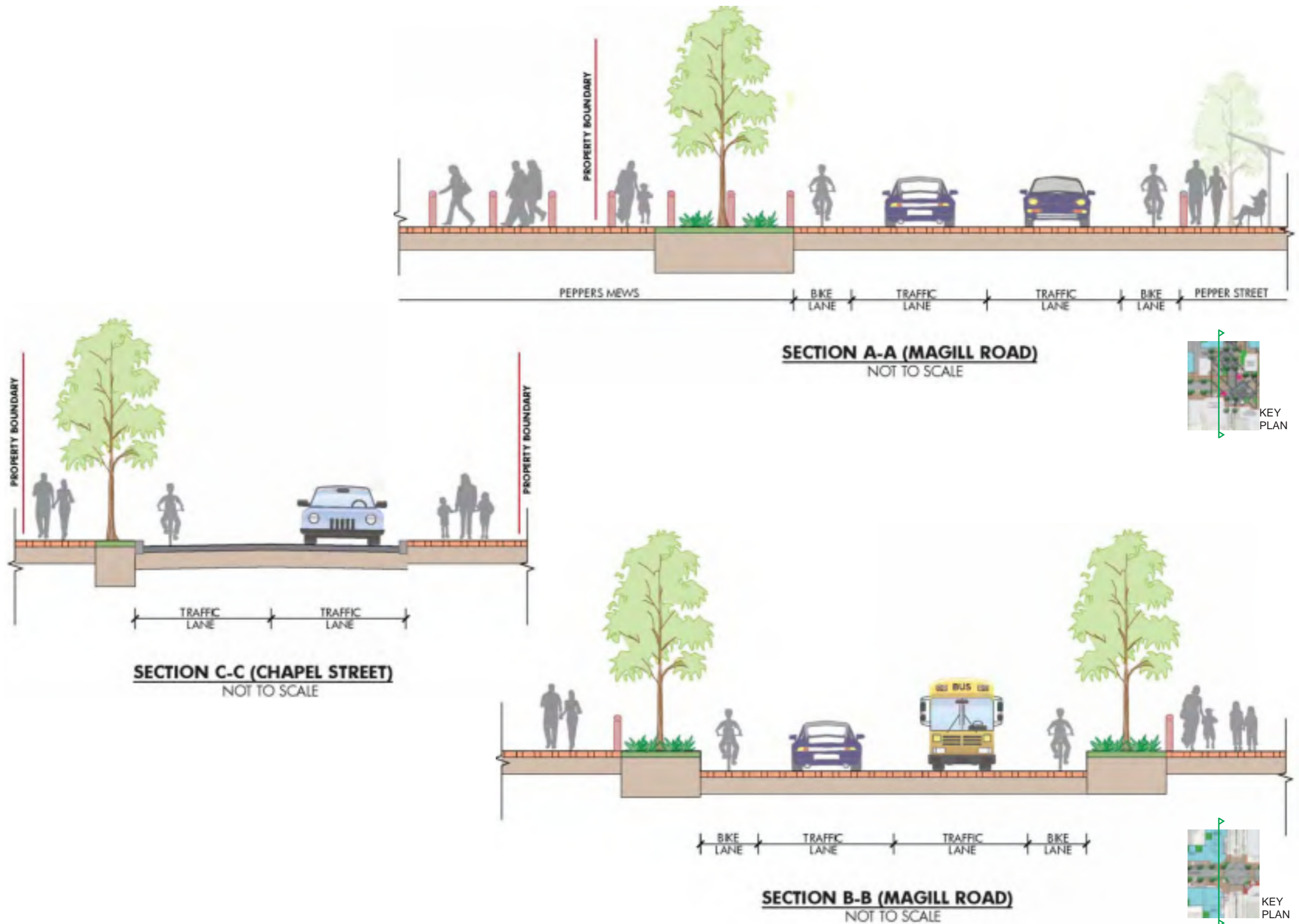


Figure 6.9 - Typical Street Sections



Figure 6.10 - Photomontage: Entering Magill Village (Travelling East from City)



**Figure 6.11 - Photomontage: New Pepper Mews Village Square**



Figure 6.12 - Photomontage: New Pepper Mews (linking Third Creek to Magill Road)





Figure 6.13 - Photomontage: Modified Intersection (St Bernards/Penfold Road)



Figure 6.14 - Photomontage: Entering Magill Village (Travelling West from Hills)

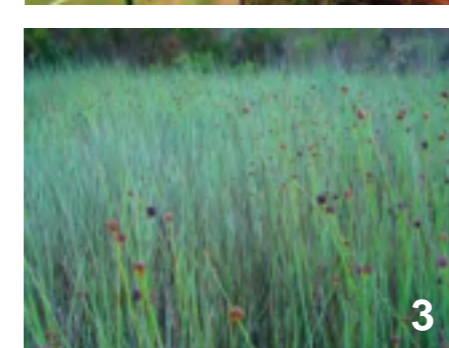
## Trees

The criteria for selection of street trees includes the ability to:-

- Tolerate harsh growing conditions
- Attain a clear trunk to at least 2.5m over pedestrian footways and 4.5m over vehicular lanes.
- Not of concern to allergy sufferers
- Have a branching habit that extend upwards from the trunk
- Have a rooting system that is unlikely to lift kerbs and paving
- Does not have a fruit or seed that is likely to create a pedestrian hazard, and is not toxic to humans and does not have spines or thorns.
- Trees can be purchased and planted at an advanced size so they are less prone to vandalism and provide an immediate visual impact
- Tolerates pruning
- Long lived
- Not susceptible to any pests or diseases commonly found in Adelaide (white fly)

### TREE SPECIES

- 1\_ *Geijera parvifolia* - Wilga
- 2\_ *Corymbia ficifolia* - Red Flower Gum
- 3\_ *Fraxinus 'Raywood'* - Claret Ash
- 4\_ *Prunus serrulata* - Cherry Plum
- 5\_ *Cupaniopsis anacardioides* - Tuckeroo
- 6\_ **VINTAGE RED™** *Eucalyptus cladocalyx 'EUC78'*



## Planting Beds

Groundcover planting will give structure and character to the streetscape. The planting palette combines both native and exotic species that have a low maintenance requirements while provide a consistent high quality contemporary appearance to the streetscape.

New groundcover planting will:

- incorporate stormwater drainage (wsud)
- enhance local amenity and ecological value
- define areas and edges
- promote safe (CPTED) public spaces

### PLANT SPECIES

- 1\_ *Lomandra longifolia* - Spiny-head Mat-rush
- 2\_ *Poa poiformis* - KINGSDALE® - Blue Tussock-grass
- 3\_ *Phormium tenax 'purpureum'* - New Zealand Flax
- 4\_ *Lomandra multiflora* - Many-Flowered Mat Rush
- 5\_ *Grevillea lavandulacea* - Lavender grevillea
- 6\_ *Westringia fruticosa MUNDI™* - Native Rosemary

## Water Sensitive Urban Design (WSUD)

There are opportunities to incorporate Water Sensitive Urban Design (WSUD) methods into the future planning and design of the Magill Village.

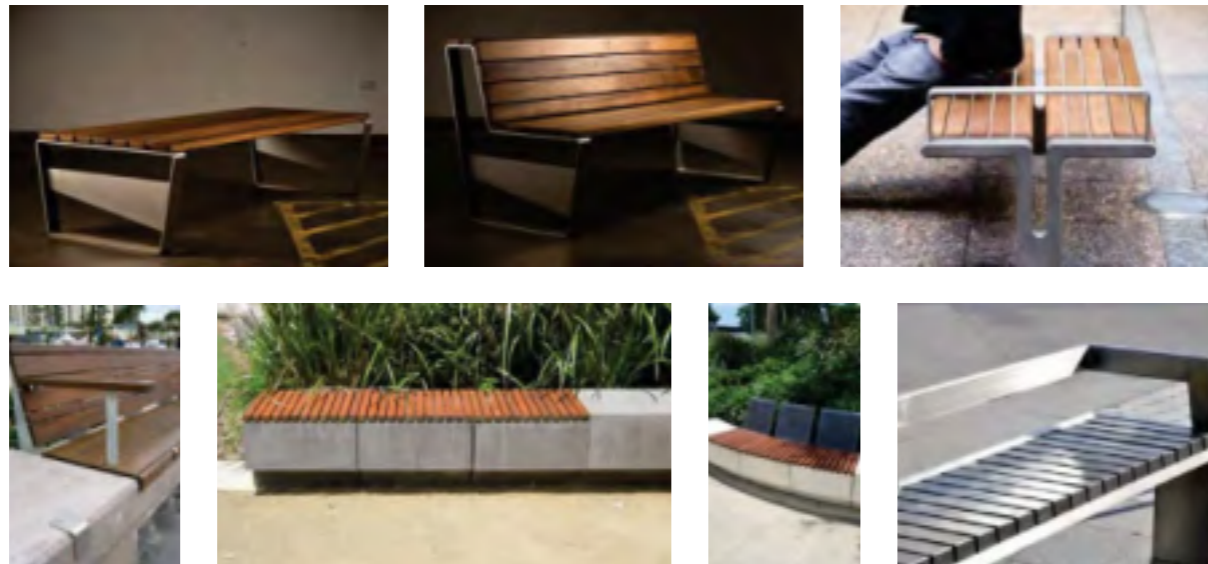
Rain gardens improve stormwater quality through use of riparian planting to filter sediment and oxygenate stormwater runoff from roadways. Stormwater runoff can also be directed to irrigate street trees.

Car parking areas can be modified to reduce the amount of impervious surfaces and incorporate rain gardens, grassed swales and vegetation to slow water flows and filter pollutants.

### PLANT SPECIES

- 1\_ *Juncus subsecundus* - Finger Rush
- 2\_ *Isolepis Nodosa* - Knobby Club-Rush
- 3\_ *Cyperus vaginatus* - Stiff Flat-Sedge
- 4\_ *Carex tereticaulis* - Sedge

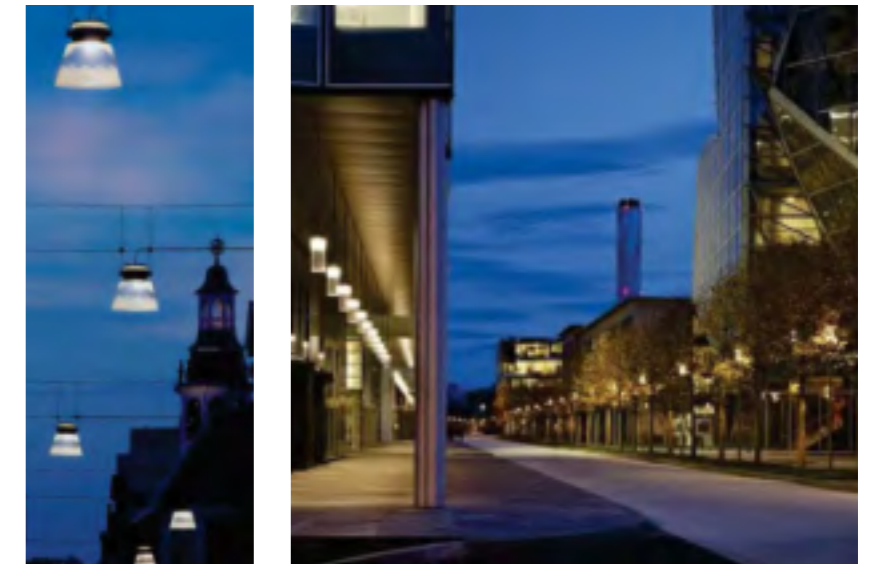
Figure 6.15 Planting Palette



SEATS / BENCH



BOLLARDS & BIKE RACKS



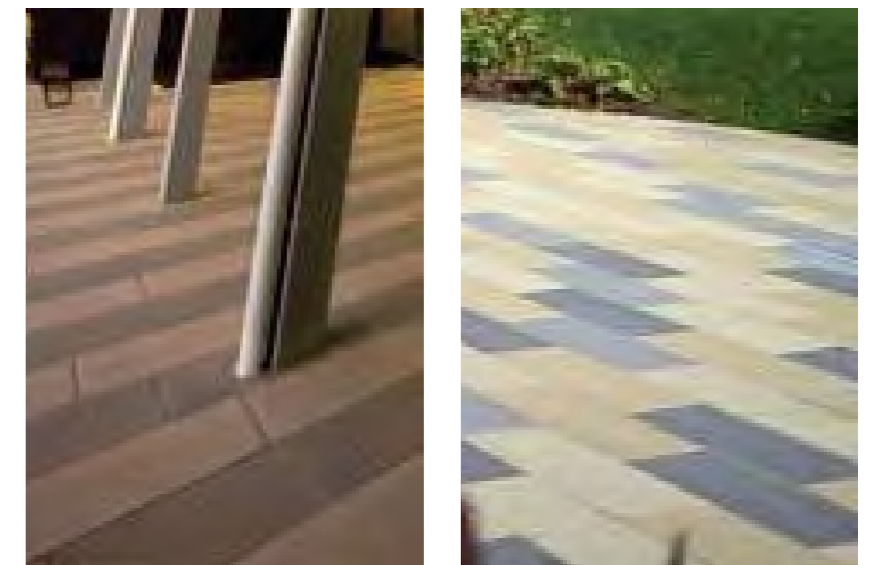
LIGHTING



PLANTER BOXES



BINS



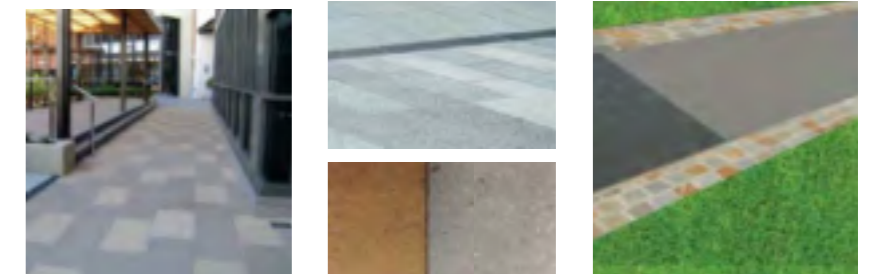
LARGE FORMAT CONCRETE PAVERS



TREE GRATES



DRINKING FOUNTAINS



CONCRETE PAVERS

GRANITE

COLOURED CONCRETE WITH STONE EDGING

Figure 6.16 - Furniture and Fixtures

## 6.2 ECONOMIC ACTIVATION

The overall success of the Magill Village project will depend heavily on the economic success of the initiatives implemented. The precinct is currently lacking in significant economic activity. This will likely compound in a few years as the University campus reconsiders its future uses, with current student levels potentially declining, but conversely new opportunities potentially emerging. The challenge will be to revitalise the Village Heart in a sustainable way that attracts, supports and retains destination businesses that will reinforce the village identity and encourage more businesses to come into the area. It is anticipated that these will focus on creative and artistic industries building on their current presence in the region.

Much of the building stock is of low quality and presents a good opportunity to be redeveloped into mixed use development containing retail, commercial, industrial and residential accommodation in line with the 30-Year Plan for Greater Adelaide.

This master plan identifies a number of development opportunities that could be completed over the time frame of the seven stages of development outlined in Section 8. It also suggests possible longer term opportunities that could be completed in the future to capitalise on the commercial and economic success of the 30-Year Plan and the revitalisation of the Magill Village. It is anticipated that these opportunities will become more viable propositions when other, broader initiatives of the 30-Year Plan are established, including key infrastructure such as improved road network for all modes of transport including public transport.

Economic activation for the precinct will likely occur in small incremental steps as the critical mass of the precinct builds. Ongoing support from the Campbelltown Food Trail, the Tour Down Under and other cultural events will be crucial to establishing and maintaining a ground swell of support for Magill Village.



**LEGEND:**

- - - Extent of Works
- Areas of potential upgrade
- Long term potential upgrade

Figure 6.17 - Economic Value



## 7.0 Image and Graphics



# 7.0 Image and Graphics

---

## 7.1 IMAGE AND GRAPHICS

The Vision and Guiding Principles established as part of this project identified that improving the image of the precinct is a key objective and will make a significant contribution to the overall success of the project.

The key principles related to improving the precinct's image are:

- Develop an Authentic Village Identity;
- Enhance Village Character;
- Reinforce the Village Centre; and
- Coordinate signage.

Included within this Masterplan is a graphic proposal for signage and branding that will reinforce these principles. An overview of the proposal is contained on the following pages. Refer to the Appendix document for full version of the proposed graphic Brand and Identity for Magill Village.



This elegant mark draws reference from Magill's winery links and the white filigree pattern draws on historical graphic references. In contrast a more contemporary typeface has been used, representing progress. With the perception that Magill Village is very much a rough diamond we have introduced the textured background pattern to again draw on hand craftsmanship and historical references. Black has been selected as it is colour neutral, it speaks of timeless qualities and allows application to eclectic businesses within the village - from light industry to fine art galleries.



Figure 7.1 - Graphic Image 1

B&W Photography



Figure 7.2 - Graphic Image 2

Colour Photography / Texture



Figure 7.3 - Graphic Image 3

Colour Textures



Figure 7.4 - Graphic Image 4

## Website



Figure 7.5 - Graphic Image 5

Street Signs



PEPPER ST

PEPPER ST. ARTS CENTRE

CEMETERY 210m

Figure 7.6 - Graphic Image 6

Interpretation Panels

# BENNETT'S MAGILL POTTERY

A privately owned family company established in 1887 by Charles William Bennett and currently run by fifth generation, John Bennett. A national supplier of some of the finest throwing terracotta and stoneware clay.



Figure 7.7 - Graphic Image 7

Banners

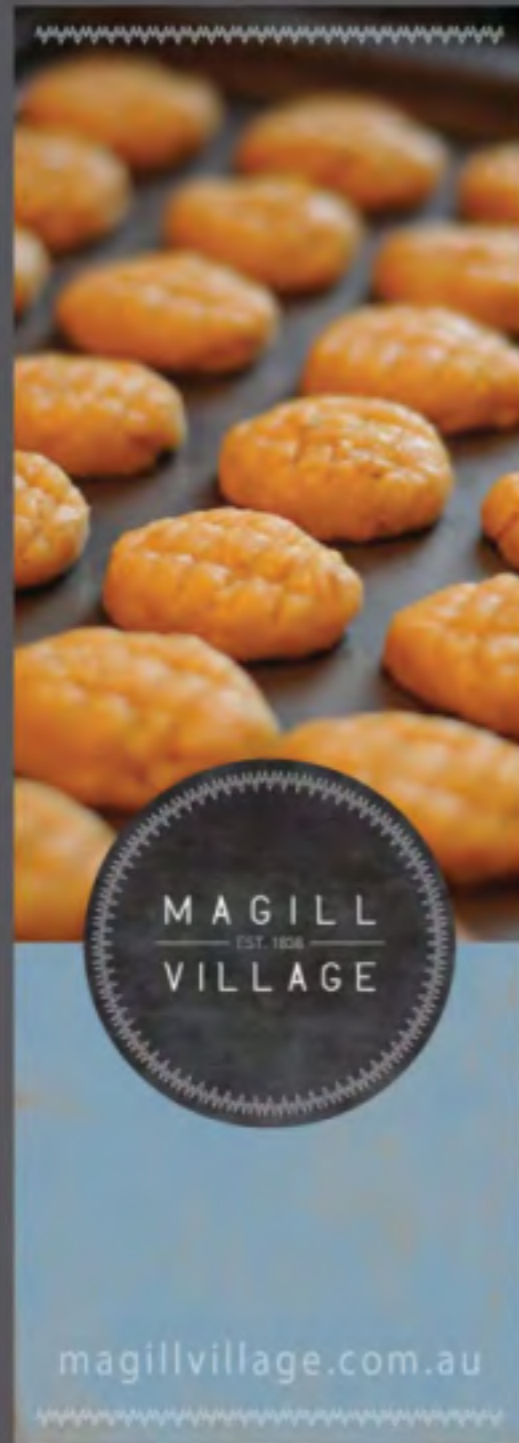


Figure 7.8 - Graphic Image 8



## 8.0 Staging



# 8.0 Staging

## 8.1 STAGING

The following staging plan has been developed and should be read in conjunction with Figure 8.1. The Staging Plan as proposed seeks to enable a logical sequence of works to optimise economic impact and catalyst.

### Stage

	<b>Initiative</b>	<b>Objective</b>
<b>1</b>	Establish pedestrian link from Magill Road to Brougham Street, along western side of Magill Institute Building. <i>(G.P.* 2.2, 3.2, 4.2 &amp; 4.3)</i>	Connect Magill Road precinct with Education Precinct and Third Creek.
<b>2</b>	Create Village Square between Pepper Street Studios and rear of Magill Institute building. <i>(G.P. 1.1, 1.2, 1.3, 1.4, 2.2 &amp; 3.2)</i>	Create central public space, and catalyst for future commercial development between Magill Road Precinct and the Education Precinct.
<b>3</b>	Upgrade Magill Road between the Village Square and Penfold Road. <i>(G.P. 1.1, 1.2, 1.3, 1.4, 2.2, 4.1, 4.2 &amp; 4.4)</i>	Improve pedestrian amenities and traffic calming.
<b>4</b>	Upgrade Magill Road between Penfold Road and the eastern boundary of the Primary School, including Ballantyne Street to 3rd Creek. <i>(G.P. 2.2, 2.3, 3.2, 3.3, 4.1 &amp; 4.2)</i>	Enhance pedestrian amenities, provide greater connection between School and education precinct. Reinforce links to passive open space to improve visual amenity.
<b>5</b>	Upgrade Magill Road west of the Village Square. <i>(G.P. 3.3)</i>	Improve pedestrian amenities and traffic calming.
<b>6</b>	Upgrade Magill Road east of the Primary School. <i>(G.P. 2.2, 3.1, 3.3 &amp; 3.4)</i>	Improve pedestrian and visual amenity.
<b>7</b>	Upgrade Pepper Street /Chapel Street. <i>(G.P. 1.2, 2.1 &amp; 3.3)</i>	Provide connection between Magill Village and key features in southern portion of the study area.

### Other Works

- Third Creek upgrade.  
*(G.P. 2.3)* Enhance connections & public amenity.
- Establish bike path along Penfold Road and St Bernard Road.  
*(G.P. 3.1)* Improve mobility.

\*G.P. = Guiding Principles (refer to Figure 5.1 - Vision and Guiding Principles for details).

In addition to the above staging, the Magill Village Masterplan process has identified the following initiatives to be undertaken by other parties that will further enhance the social, cultural and economic success of the Study Area. The Stages above have been structured and ordered to allow maximum facilitation of these initiatives.

- Education Precinct - Active Play Zone
- Education Precinct - Regional Sports Zone
- Education Precinct - University Zone - Adaptive Re-use
- Education Precinct - Mixed use development
- Magill Precinct - Mixed commercial development linking the Village Square and Education Precinct, commercial and residential.
- Third Creek upgrade
- Upgrades to other streets using the principles established for Chapel/Pepper Street. Possible streets for initial consideration include, Brougham Street, Lorne Avenue and Ellis Road.
- Sports field east of St Bernard Road. Extended regional sports zone and/or medium density residential.

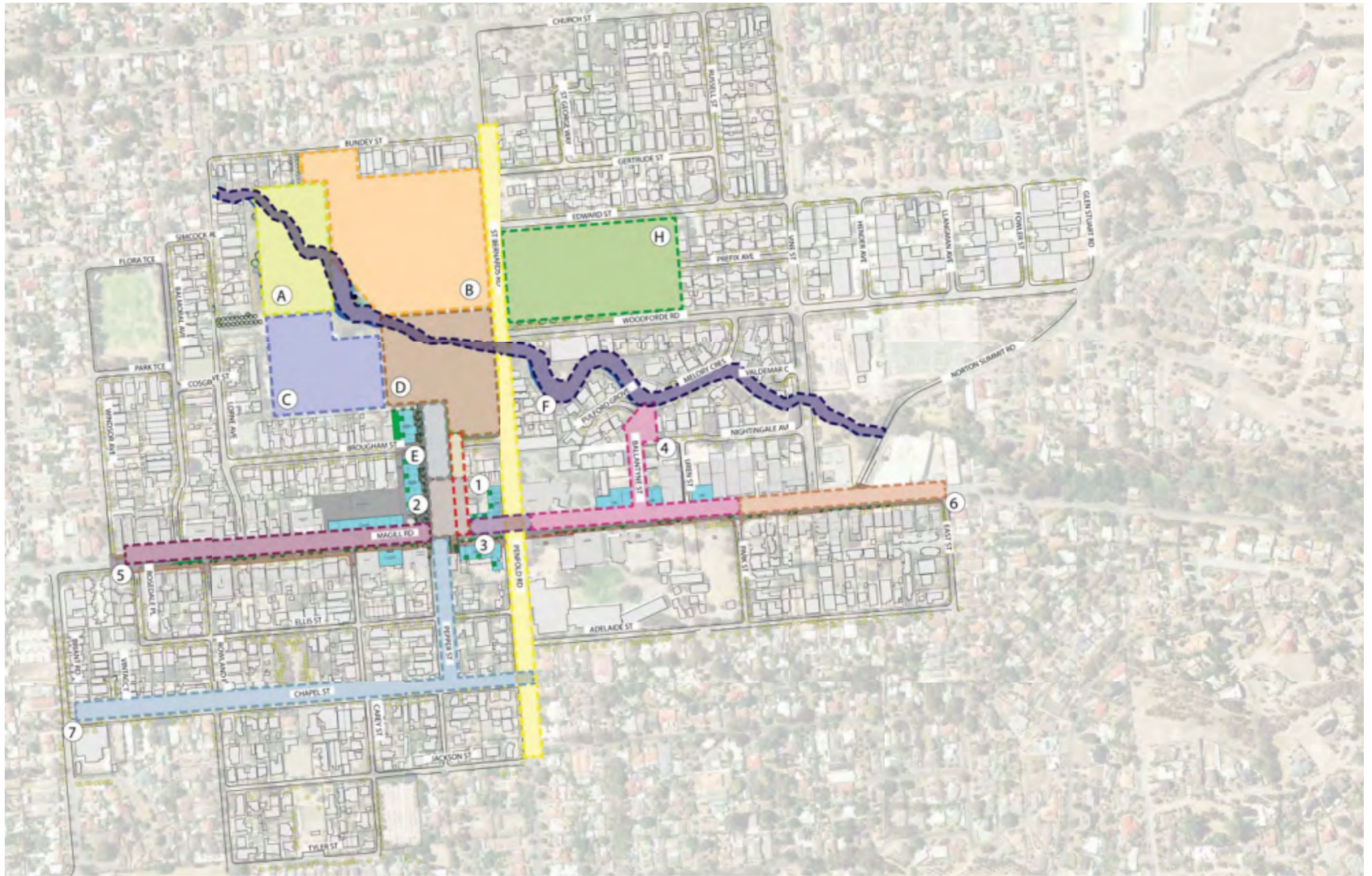
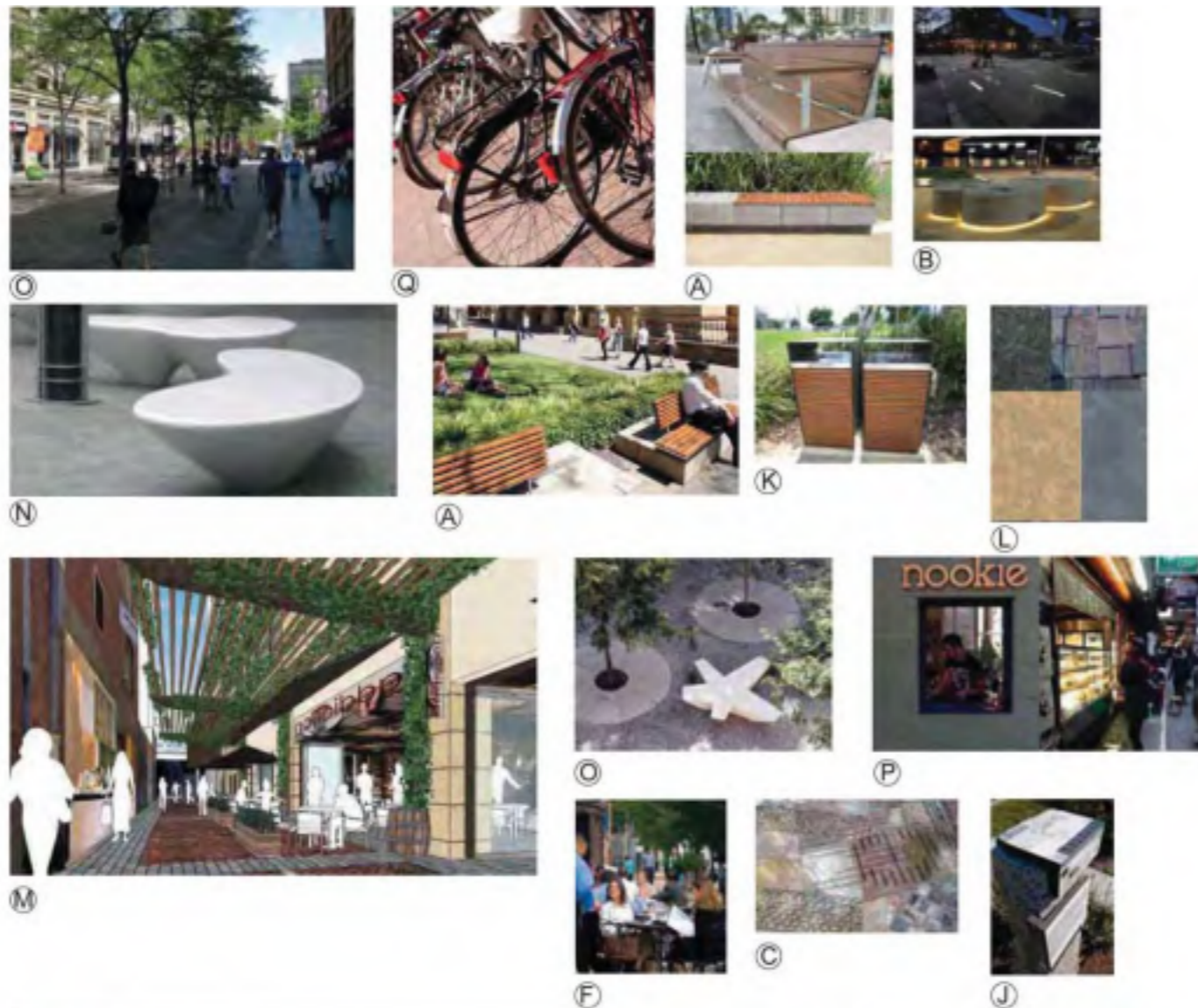


Figure 8.1 - Staging Plan



**LEGEND**

- Extent of upgrade
- Proposed new trees (*Fraxinus angustifolia* - Claret Ash Tree & *Corymbia ficifolia* - Red Flower Gum).
- Proposed mixed use commercial/residential opportunities
- ✱ Focal point for public art
- New garden beds. May act as a bio-retention filter to improve stormwater runoff. Also provides a more pedestrian scale and focus to the streetscape
- Proposed bike lane (1.2m wide)
- Existing kerb to be removed
- Footpath - Existing footpath widened where possible. New paving

- (A)** New public seating integrated with raised planter beds
- (B)** Feature uplighting
- (D)** Signalised crossing moved to define Village square
- (F)** Outdoor dining opportunity
- (G)** Bollards to align with corridor of trees
- (J)** Cultural & Heritage walk marker (QR Code)
- (K)** Rubbish bin
- (M)** Greenery introduced with outdoor dining to provide more intimate spaces within the square
- (N)** Outdoor seating to introduce elements of public art within the square and streetscape
- (O)** New trees within the hardscape are aligned to create a more formal and pedestrian friendly space
- (P)** Opportunities for pop up restaurants/ coffee shops activating blank walls



Figure 8.2 - Staging Plan (Stage 1 - Village Square)

## 9.0 Conclusion



# 9.0 Conclusion

---

## 9.1 CONCLUSIONS AND RECOMMENDATIONS

Responsibility for implementing the Magill Village Masterplan is split between Campbelltown City Council and City of Burnside. The precinct and its immediate environs represent a key development area for concentrated growth and intensified development under the State Government's 30-Year Plan for Greater Adelaide. This will bring with it higher density living and less reliance on car transport and increased use of more sustainable transit options. All of these initiatives support the creation of the Magill Village where the daily social, business, recreational, education and retail needs of the community are provided. In order for the outcomes described in this Masterplan to become reality, sustained and unwavering commitment from all key stakeholders will be required. This must be led by both Councils working collaboratively with common goals.

## 9.2 THE NEXT STEP

The Masterplan process has identified the following projects that could be undertaken as a next step in the realisation of the Magill Village vision. Implementation of any of these will be subject to available funding. In some cases further scoping and project definition would need to be undertaken to confirm viability and anticipated benefits arising from them.

1. Undertake detailed concept design options for Stage 1 Pepper St Mews link to assist with defining and negotiating either right-of-way or land purchase agreement with current Institute Building owners. This would include more a detailed feasibility study to establish accommodation mix, floor areas and potential occupants to develop a design outcome that would be suitable to all parties and one that is consistent with the 30-Year Plan for Greater Adelaide.
2. Detailed Graphic Design for enhancing precinct identity. This may include detailed design for way-finding signage, branding banners and interpretive initiatives. This would provide the catalyst to cement the branding within the precinct and create the impetus for further works.
3. Development of marketing and promotional information to local businesses and stakeholders, as a follow up to the release of the Masterplan. This would serve to keep the Masterplan in the public's mind and would further reinforce the Magill Village brand and desired identity.

4. Detailed landscape and civil engineering design analysis for Magill Road (from Pepper Street to St Bernards/Penfolds Road) to resolve in-principle the optimum pavement design levels, road carriageway levels, kerb design, storm water management, introduction of trees, management of underground services including future PLEC, etc. This will assist with informing DPTI on future profile changes to Magill Road in order to more effectively manage the existing levels of Magill Road.





**COMPLETE**